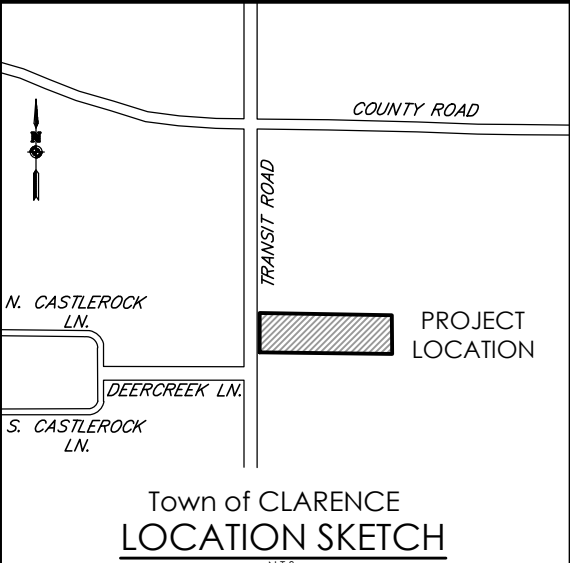
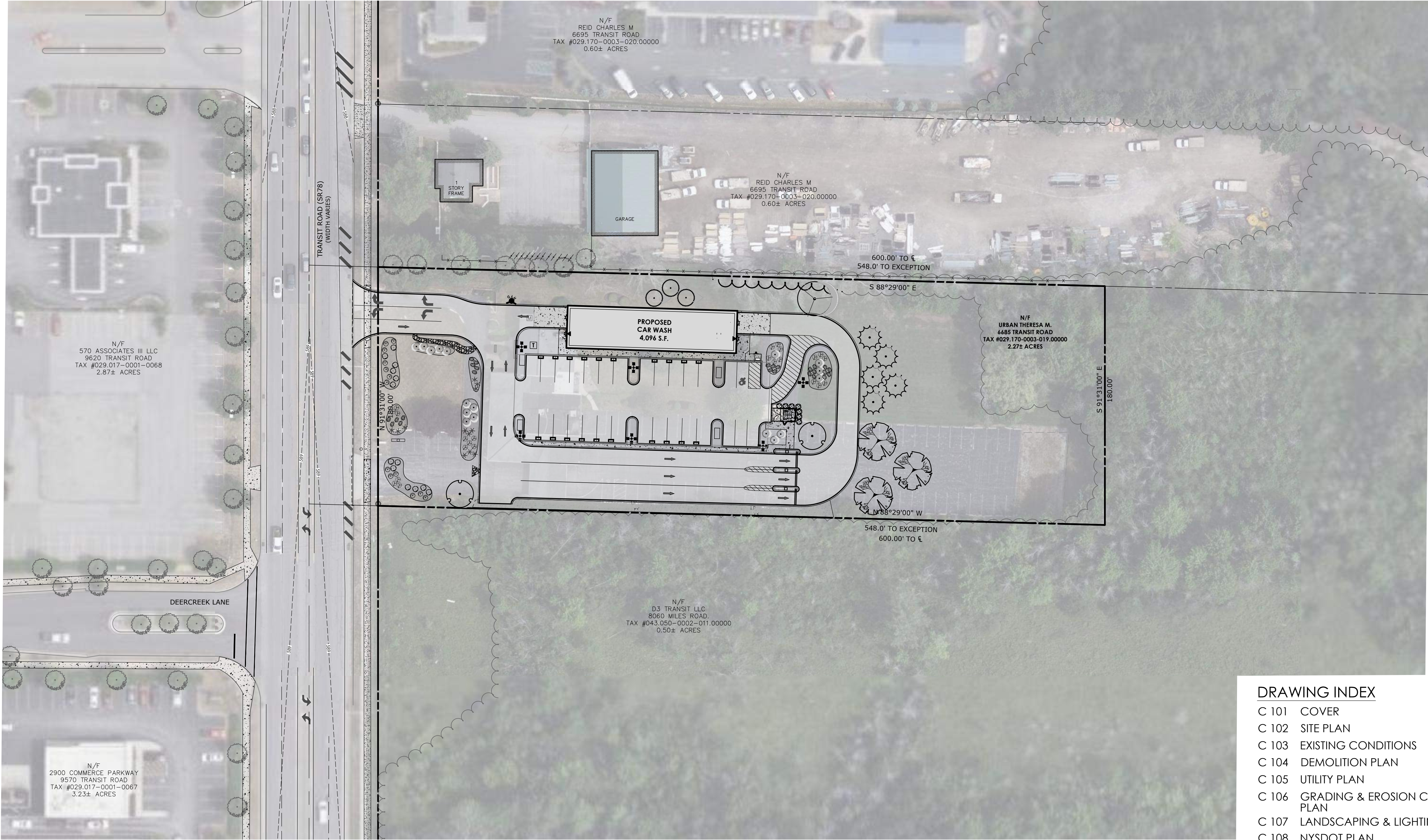
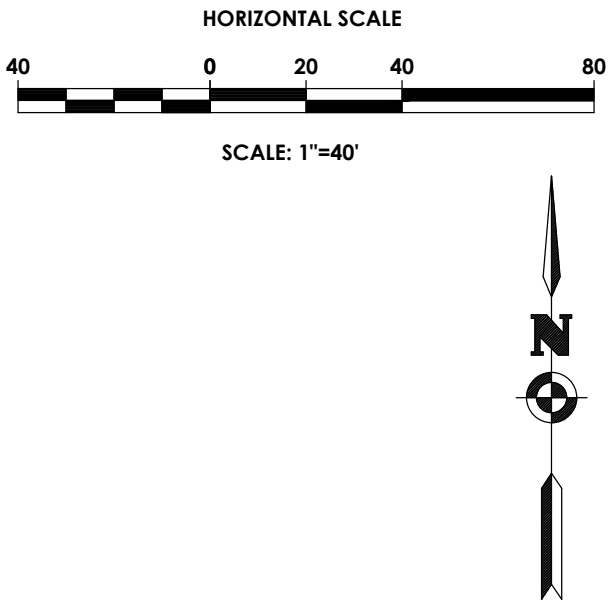


SITE PLANS FOR  
ROYAL WASH DEVELOPMENT - CLARENCE  
6685 TRANSIT ROAD  
TOWN OF CLARENCE, ERIE COUNTY, NEW YORK  
P.N. 20213204.0001



Client:  
**DANIELE FAMILY COMPANIES**  
2851 MONROE AVENUE  
ROCHESTER, NY 14618

**PASSERO ASSOCIATES**  
242 West Main Street Suite 100  
Rochester, New York 14614  
Principal-in-Charge: Jess Sudol, PE  
Project Manager: David Cox, PE  
Designed by: James Ritzenthaler



Revisions			
No.	Date	By	Description
1	11/01/21	ABC	PER ECWA COMMENT
2	01/24/22	ABC	PER TOWN COMMENT
3	04/14/22	ABC	PER NYSDOT COMMENTS
4	10/31/22	ABC	FULL ACCESS DRIVEWAY PER NYSDOT
5	12/05/22	ABC	PER TOWN ENGINEER COMMENTS

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COVER

RWD CLARENCE

Town/City: CLARENCE  
County: ERIE State: NEW YORK

Project No.  
**20213204.0001**

Drawing No. Sheet No.  
**C 101 1**

Scale:  
**1" = 40'**

Date  
**SEPTEMBER 2021**

DRAWING INDEX

- C 101 COVER
- C 102 SITE PLAN
- C 103 EXISTING CONDITIONS
- C 104 DEMOLITION PLAN
- C 105 UTILITY PLAN
- C 106 GRADING & EROSION CONTROL PLAN
- C 107 LANDSCAPING & LIGHTING PLAN
- C 108 NYSDOT PLAN
- C 201-209 DETAILS





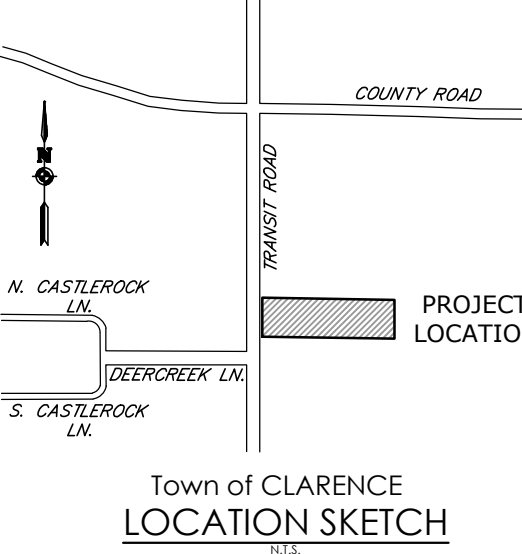


ABBREVIATION TABLE	
A.G.	ABOVE GROUND
C.I.	CAST IRON
CNC.	CONCRETE
E.O.P.	EDGE OF PAVEMENT
MEAS.	MEASURES
R.O.W.	RIGHT OF WAY
STP.	STANDPIPE
STL.	STEEL
TRANS.	TRANSFORMER
UDT.	UNDETERMINED

LEGEND	
	CATCHBASIN
	CLEANOUT (UNKNOWN TYPE)
	CLEANOUT DRAINAGE SEWER
	CLEANOUT SANITARY SEWER
	END SECTION DRAINAGE PIPE
	GAS VALVE
	HYDRANT
	LIGHTPOLE
	MANHOLE (UNKNOWN TYPE)
	MANHOLE ELECTRIC
	MANHOLE DRAINAGE INLET
	MANHOLE DRAINAGE SEWER
	MANHOLE SANITARY SEWER
	SIGN POST (SINGLE)
	TRAFFIC LIGHT SPAN POLE
	UTILITY POLE
	UTILITY POLE ANCHOR WIRE
	UTILITY POLE WITH LIGHT
	WATER SERVICE
	WATER VALVE

Revisions			
No.	Date	By	Description

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## Passero Associates

242 WEST MAIN STREET, SUITE 100 (585) 325-1001  
ROCHESTER, NY 14614 Fax: (585) 760-8581

Principal-in-Charge David L. Cox, PE  
Project Manager Robert A. Vento, PLS  
Drafted by R.D.C.

Client:  
THE DANIELE FAMILY COMPANIES  
2851 MONROE AVENUE  
ROCHESTER, NY, 14618

TOPOGRAPHIC SURVEY OF  
LANDS NOW OR FORMERLY  
OWNED BY GEORGE & TERESA  
URBAN

6685 TRANSIT ROAD

TAX ACCT. NO. 029.170-0003-19

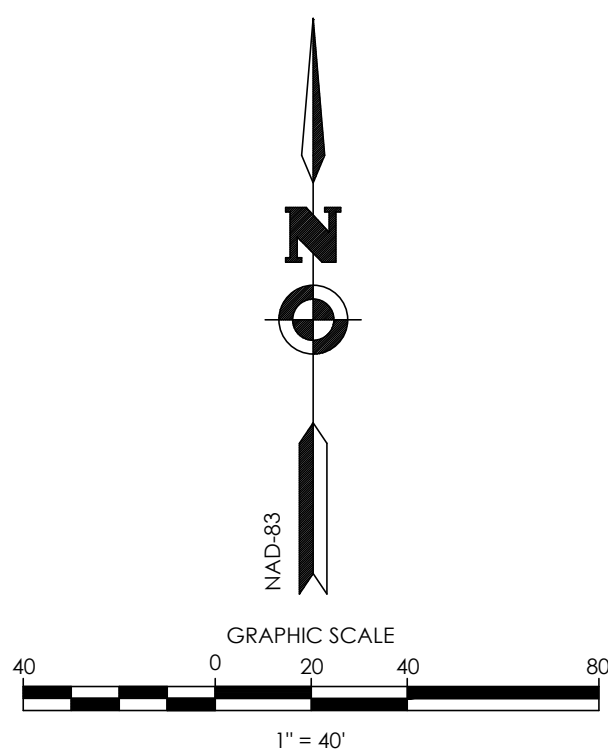
TOWN OF CLARENCE, ERIE COUNTY,  
NEW YORK STATE

Project No.  
20213204.0001

Drawing No.	Sheet No.
C-103	3

Scale:  $1'' = 40'$

Date 01.19.2022



SURVEY NOTES:

1. THE HORIZONTAL DATUM HEREON IS REFERENCED TO THE N.Y.S. PLANE COORDINATE SYSTEM, WESTERN ZONE, TRANSVERSE MERCATOR SYSTEM, NAD 83 (2011 ADJ.). USING RTK GPS FROM THE NYSNET CONTINUOUSLY OPERATING REFERENCE STATIONS.
2. ELEVATIONS SHOWN HEREON ARE REFERENCED TO NAVD 88 ELEVATIONS USING RTK GPS FROM THE NYSNET CONTINUOUSLY OPERATING REFERENCE STATIONS.
3. DISTANCES SHOWN HEREON ARE GROUND DISTANCES. BEARINGS AND COORDINATES ARE REFERENCED TO GRID.
4. SURVEY FIELD WORK ON THIS SURVEY WAS DONE TO AN ACCURACY GREATER THAN ONE PART IN 10,000, (1:10,000), USING MONUMENTS REFERENCED HEREON.

REFERENCE:

1. INSTRUMENT SURVEY PREPARED BY STEVE A CARVER PLS, 6685 TRANSIT ROAD, DATED JUNE 7, 2021.
2. ABSTRACT BY CHICAGO TITLE, NO.2113-3243SCH, DATED JULY 9, 2021.
3. FEMA FLOOD INSURANCE RATE MAP FOR THE TOWN OF CLARENCE, ERIE COUNTY, NEW YORK, COMMUNITY PANEL 36029C0087H, EFFECTIVE DATE JUNE 7, 2019.

## HATCH K

AREA WITHIN THE 100 YEAR  
FLOOD PLAIN BASE FLOOD  
ELEVATION (BFE)=588' (REF.3)

CERTIFICATION:

WE, PASSERO ASSOCIATES, CERTIFY THAT THIS MAP WAS PREPARED USING PORTIONS OF THE REFERENCE MATERIAL AS LISTED HEREON AND FROM NOTES OF AN TOPOGRAPHIC SURVEY COMPLETED JULY 26, 2021. THIS PARCEL IS SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF RECORD. NO CERTIFICATION IS EXTENDED TO INFORMATION NOT REFERENCED.



ROBERT A. VENTO N.Y.S.P.L.S. NO. 049701 DATE  
bvento@passero.com

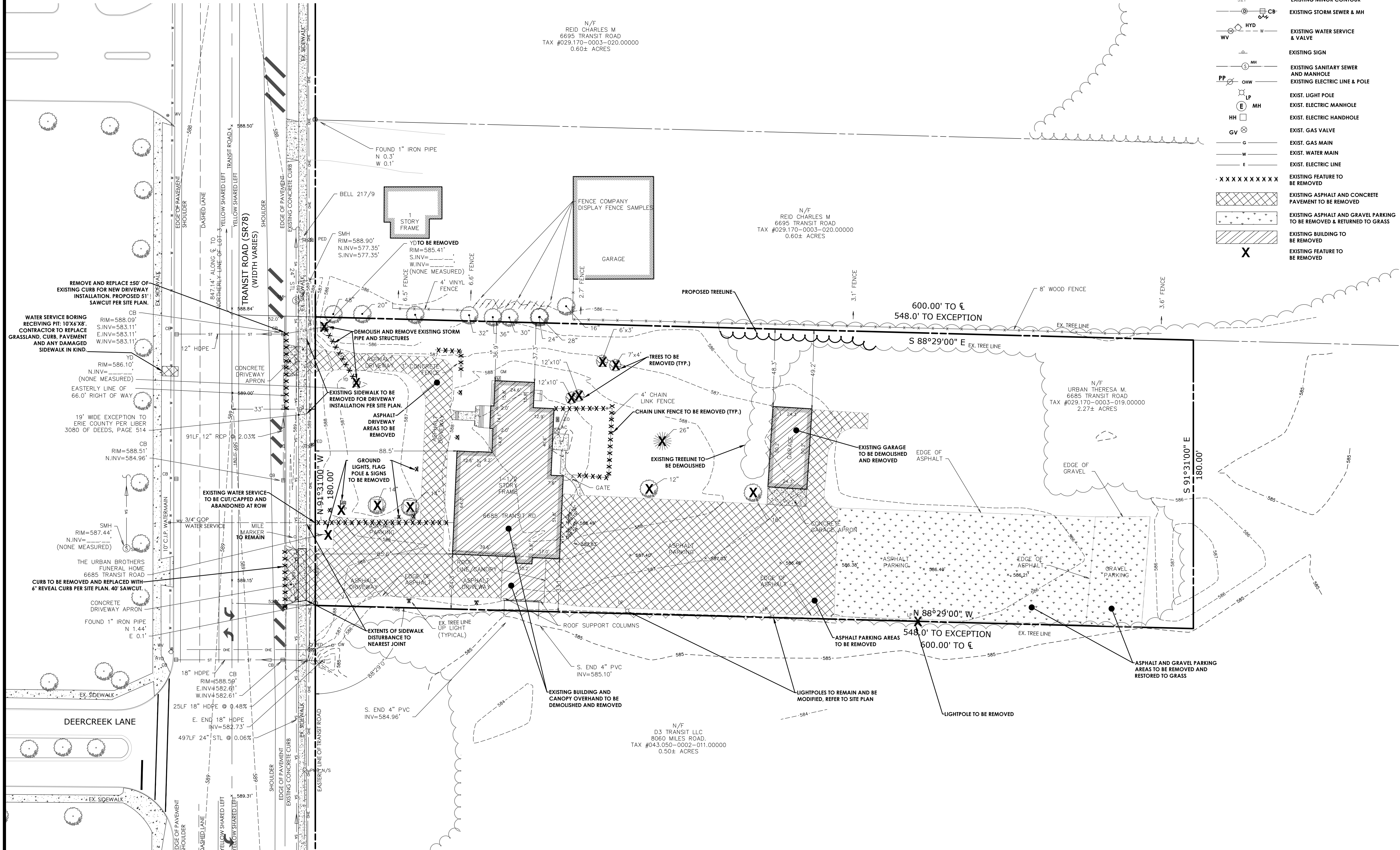
ONLY COPIES FROM THE ORIGINAL OF THIS SURVEY MARKED  
WITH AN ORIGINAL INKED OR EMBOSSED SEAL AND INKED  
SIGNATURE SHALL BE CONSIDERED A TRUE AND VALID COPY.

CERTIFICATION INDICATED HEREON SHALL RUN ONLY TO THE PERSON FOR WHOM THE SURVEY IS PREPARED, AND ON HIS BEHALF TO THE AGENCIES LISTED HEREON. CERTIFICATIONS ARE NOT TRANSFERABLE TO ADDITIONAL INSTITUTIONS OR SUBSEQUENT OWNERS.

\\PASSERO.COM\DFS\PROJECTS-NEW\2021\2021 3204\20213204.0001\01 CAD\_BIM\_MODELS\SURVEY\20213204.0001 RCW CLARENCE BOUNDARY AND TOPO FOR DESIGN GPS NYSNET 3D2019.DWG 1/19/2022 12:37 PM

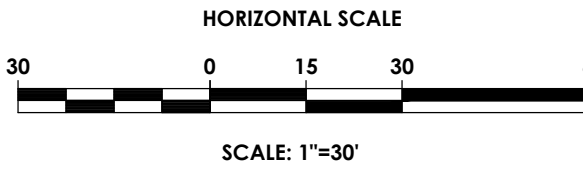


Y:\PROJECTS-NEW\2021\20213204\20213204.0001\01 CAD BIM MODELS\CIVIL\20213204.0001 - EXCON.DWG 1/17/2023 10:41 AM James Ritzenholler

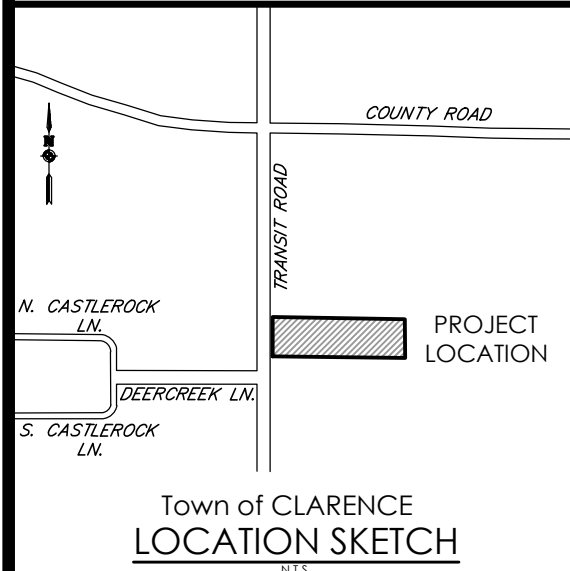


LEGEND - DEMO:

	PROPERTY BOUNDARY
	R.O.W.
	EXISTING CENTER LINE ROAD
	EXISTING BUILDING
	EXISTING FENCE
	EXISTING EASEMENT LINE
	SETBACK
	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR
	EXISTING STORM SEWER & MH
	EXISTING WATER SERVICE & VALVE
	EXISTING SIGN
	EXISTING SANITARY SEWER AND MANHOLE
	EXISTING ELECTRIC LINE & POLE
	EXIST. LIGHT POLE
	EXIST. ELECTRIC MANHOLE
	EXIST. ELECTRIC HANDHOLE
	EXIST. GAS VALVE
	EXIST. GAS MAIN
	EXIST. WATER MAIN
	EXIST. ELECTRIC LINE
	EXISTING FEATURE TO BE REMOVED
	EXISTING ASPHALT AND CONCRETE PAVEMENT TO BE REMOVED
	EXISTING ASPHALT AND GRAVEL PARKING TO BE REMOVED & RETURNED TO GRASS
	EXISTING BUILDING TO BE REMOVED
	EXISTING FEATURE TO BE REMOVED



**PA**  
PASSERO ASSOCIATES  
engineering architecture



Client:  
**DANIELE FAMILY COMPANIES**  
2851 MONROE AVENUE  
ROCHESTER, NY 14618

**PASSERO ASSOCIATES**

242 West Main Street Suite 100  
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(585) 325-1000  
Fax: (585) 325-1691  
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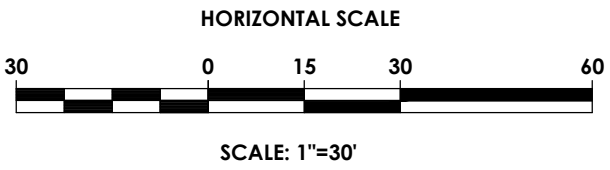
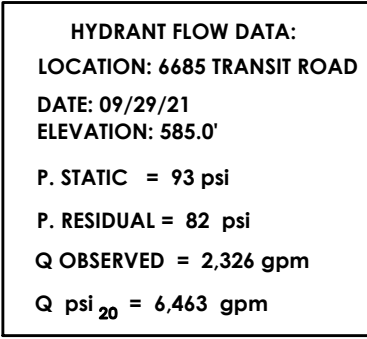
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**DEMOLITION PLAN**  
**RWD CLARENCE**

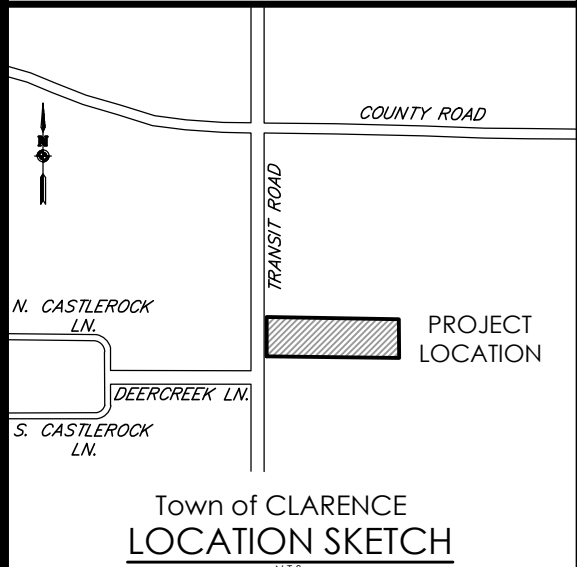
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County: ERIE	State: NEW YORK
Project No. <b>20213204.0001</b>	
Drawing No. <b>C 104</b>	Sheet No. <b>4</b>
Scale: <b>1" = 30'</b>	
Date <b>SEPTEMBER 2021</b>	

NOT FOR CONSTRUCTION





	PROPERTY BOUNDARY
	R.O.W.
	EXISTING CENTER LINE ROAD
	EXISTING BUILDING
	EXISTING FENCE
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	PROPOSED BUILDING
	PROPOSED CONCRETE
	PROPOSED SIGN
	PROPOSED LIGHT
	PROPOSED STORM SEWER, INLET MH, CB & END SECTION
	EXISTING STORM SEWER & MH
	PROPOSED WATER SERVICE W/ HYDRANT & VALVE
	EXISTING WATER SERVICE & VALVE
	PROPOSED SANITARY SEWER AND MANHOLE
	EXISTING SANITARY SEWER AND MANHOLE
	EXISTING ELECTRIC LINE & POLE
	EXIST. LIGHT POLE
	EXIST. ELECTRIC MANHOLE
	EXIST. ELECTRIC HANDHOLE
	EXIST. GAS VALVE
	EXIST. GAS MAIN
	EXIST. WATER MAIN
	EXIST. ELECTRIC LINE
	PROPOSED UNDERGROUND ELECTRIC



Client:

DANIELE FAMILY  
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UTILITY PLAN

RWD CLARENCE

Town/City: CLARENCE  
County: ERIE State: NEW YORK

Project No. 20213204.0001

Drawing No.	Sheet No.
C 105	5

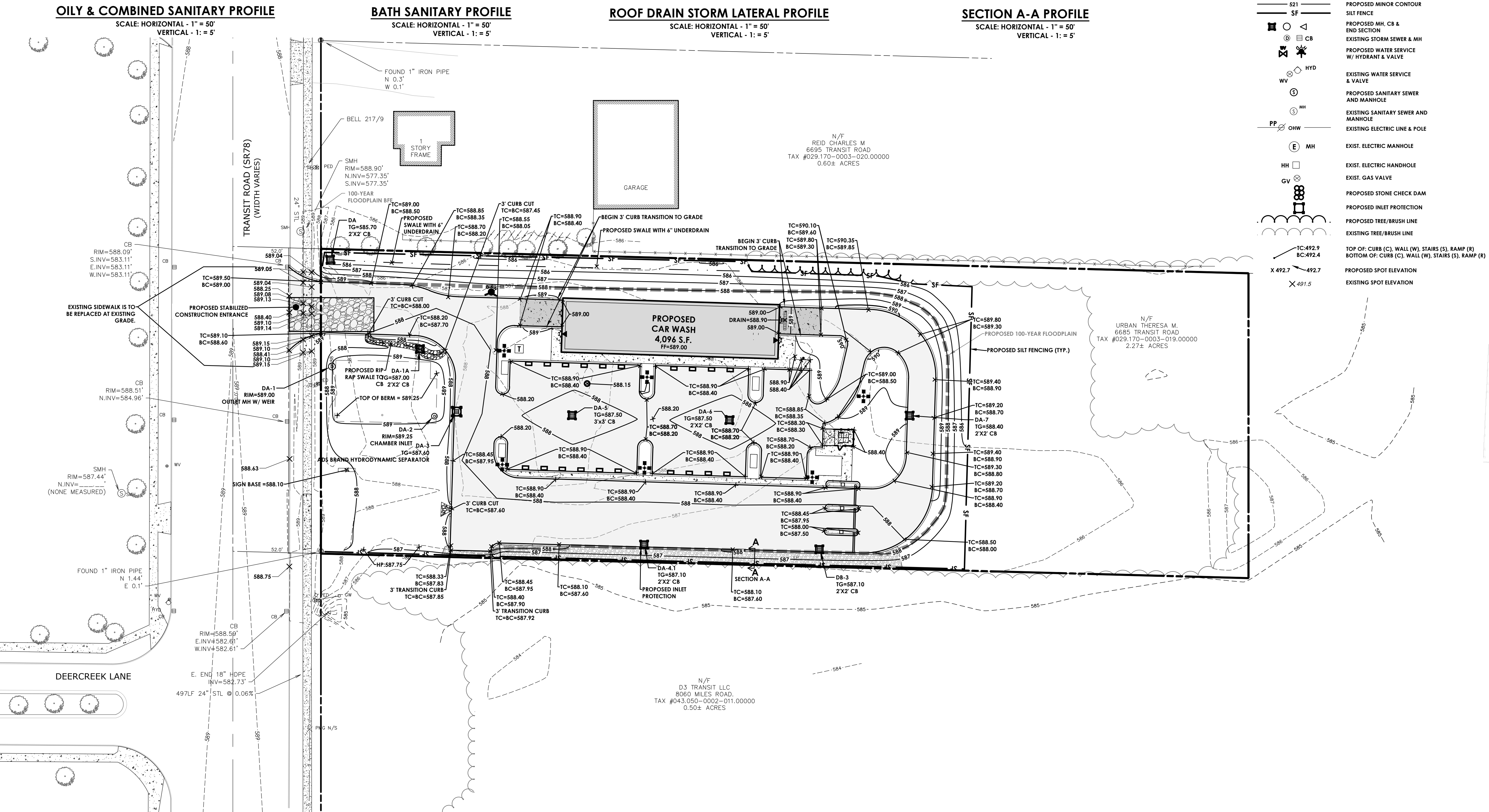
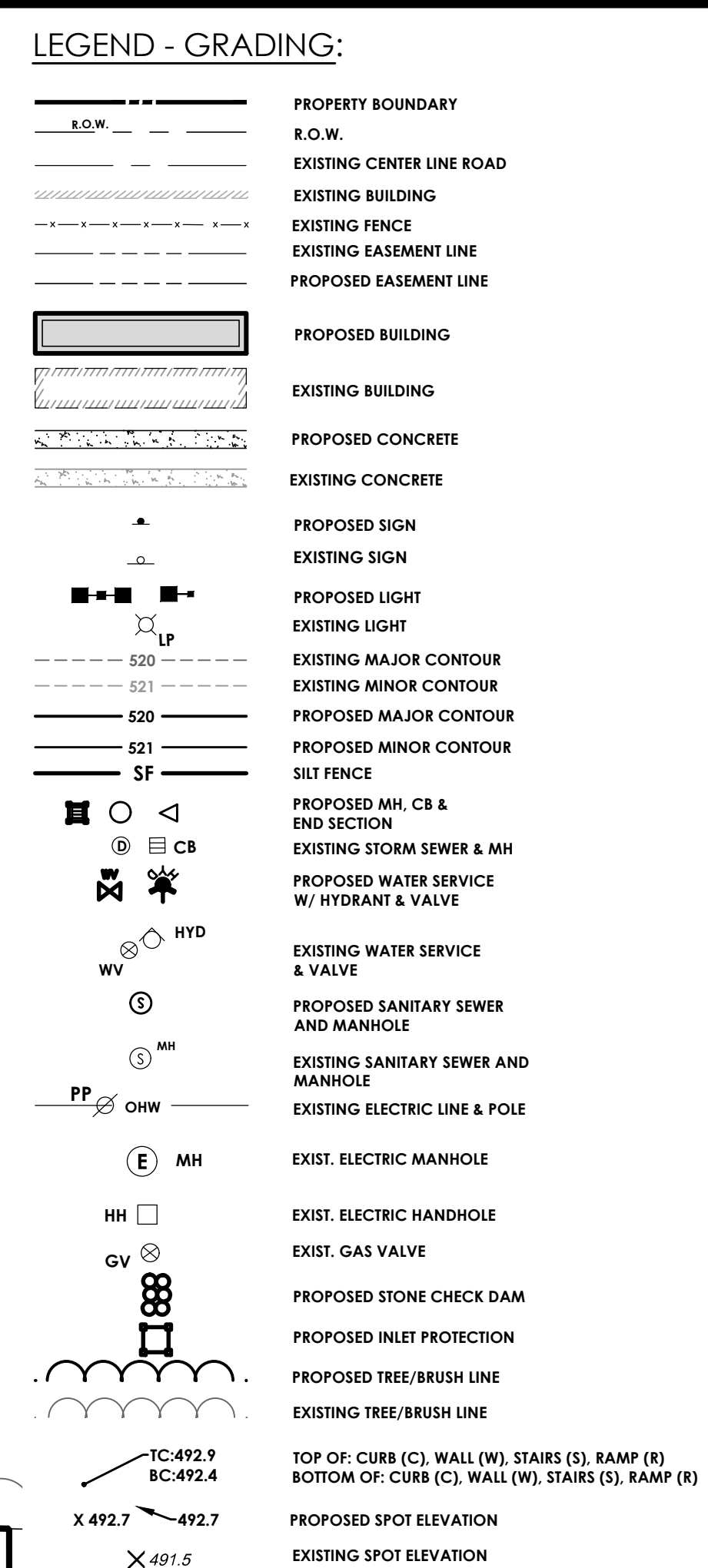
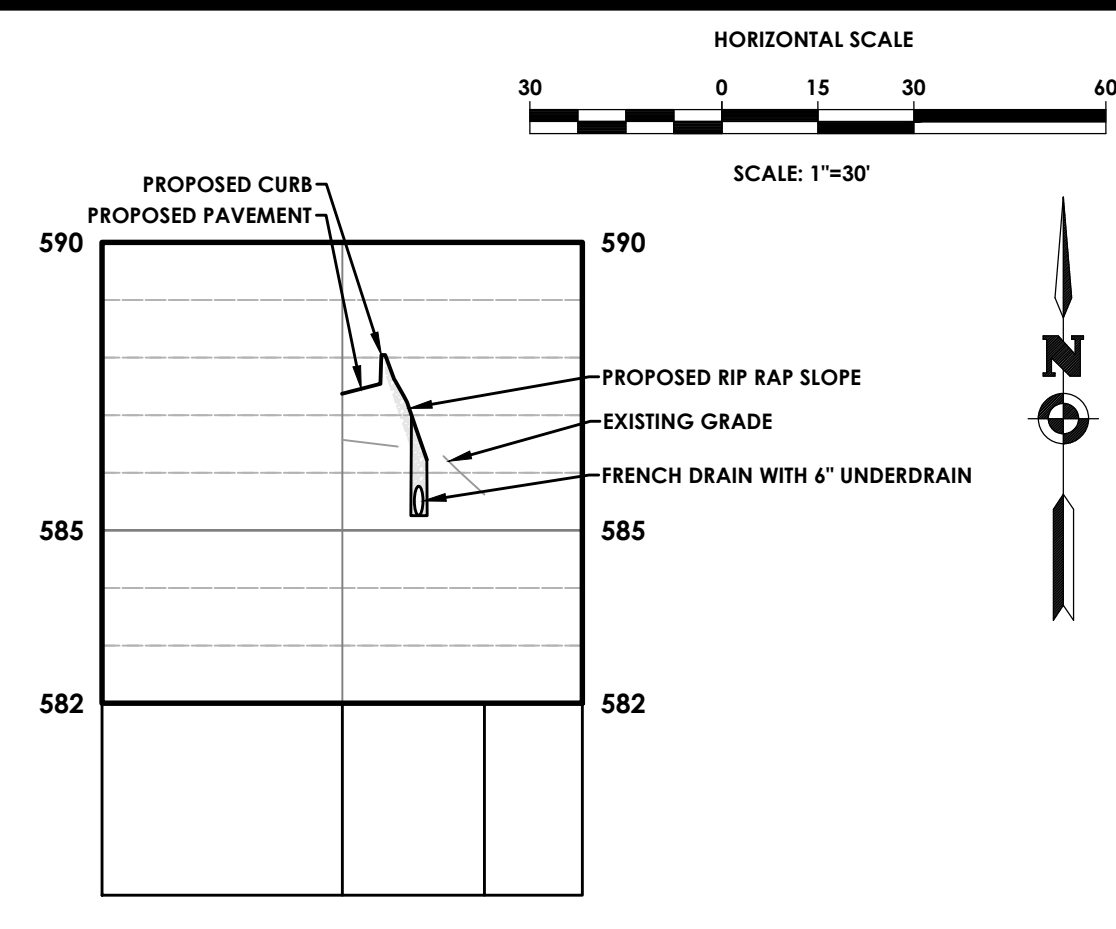
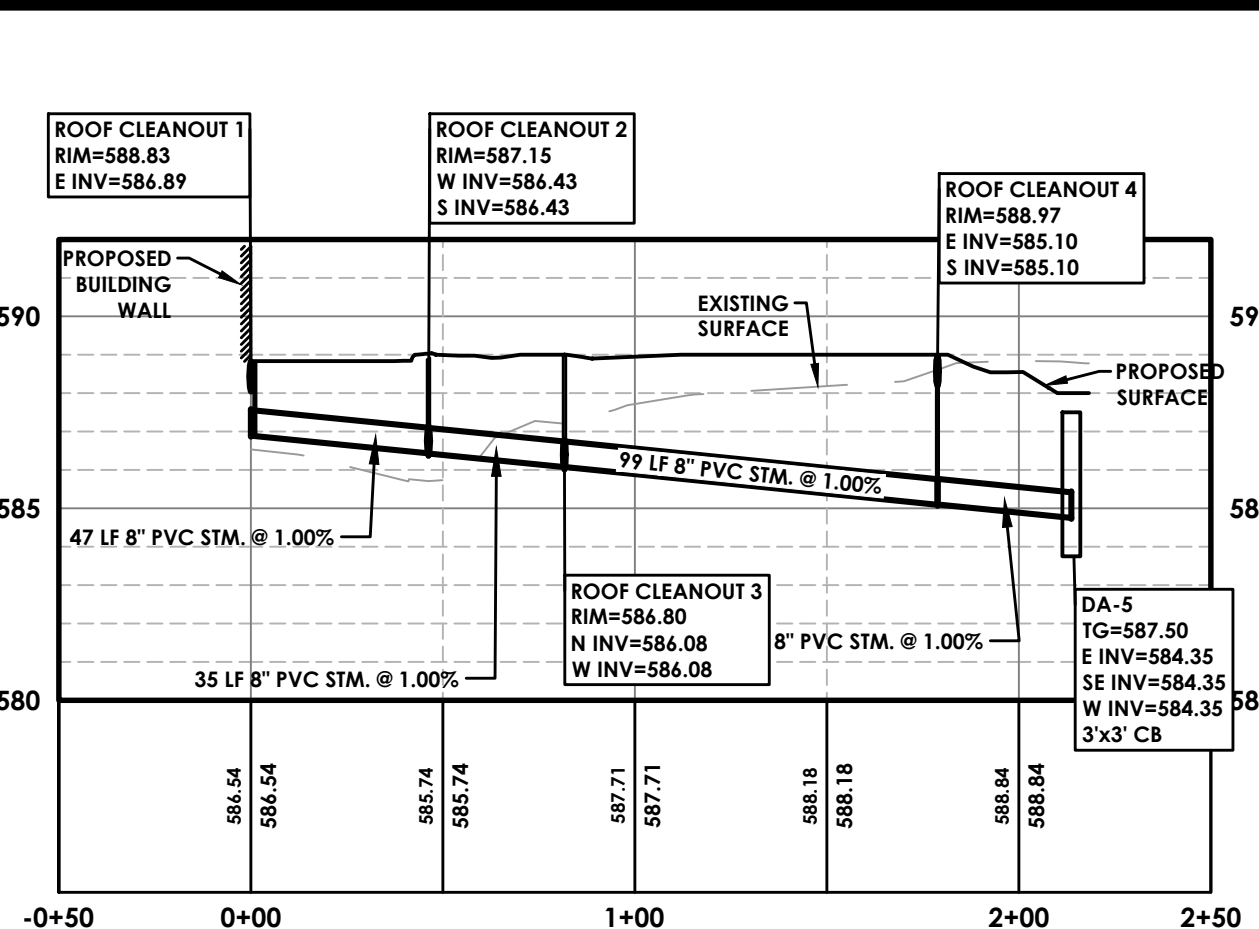
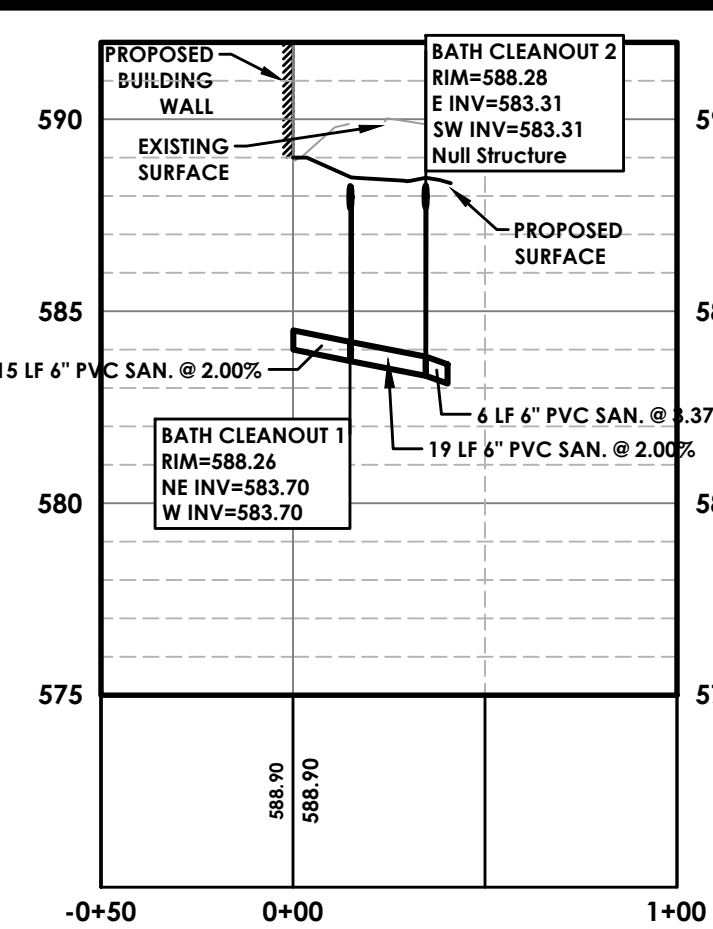
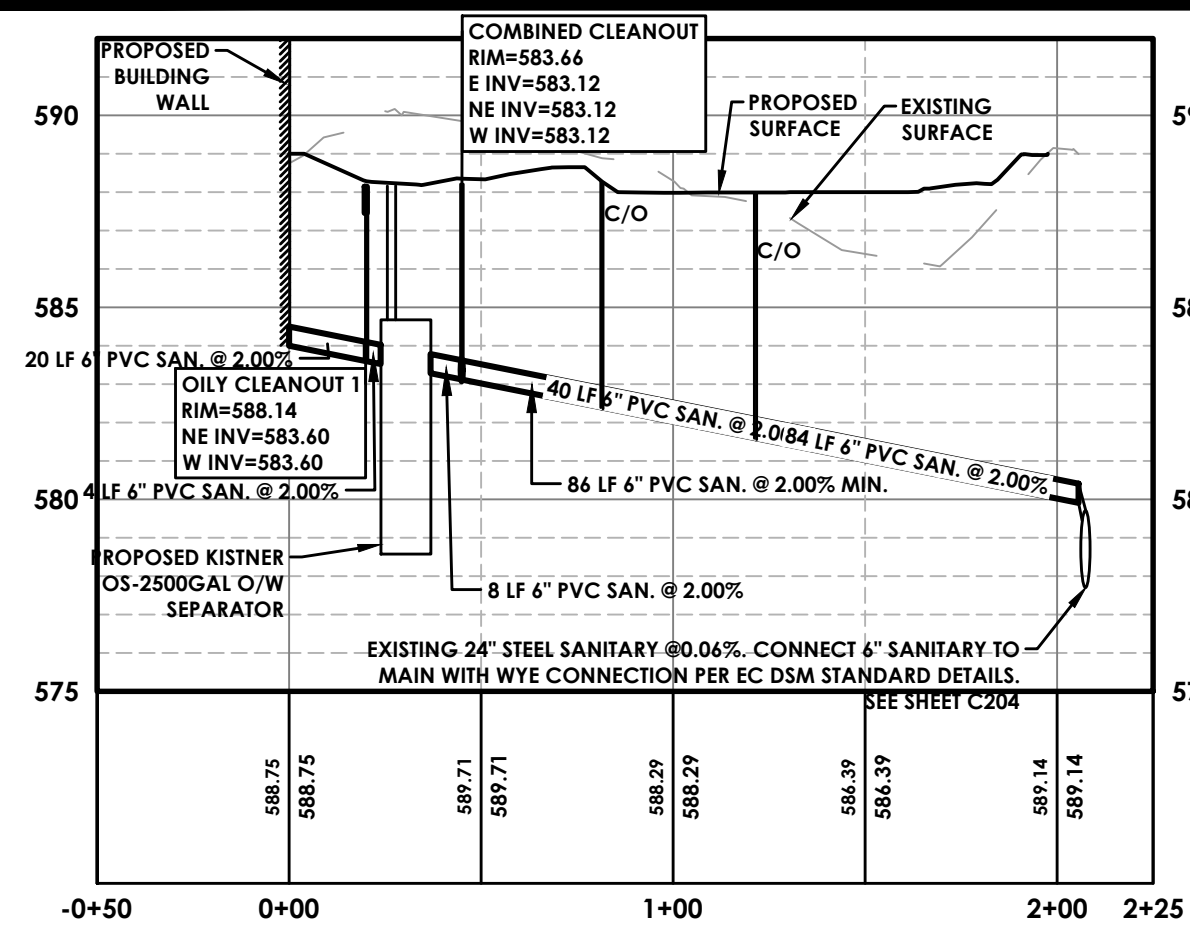
Scale:

$1'' = 30'$

Date  
SEPTEMBER 2021

NOT FOR CONSTRUCTION







\*Seed Mix B only when you have wet-occasional wet locations.

### TOPSOIL AND SEEDING NOTES:

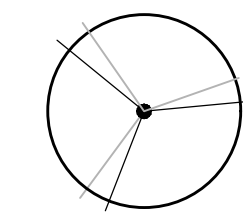
- THE EARTHWORK CONTRACTOR IS RESPONSIBLE FOR ROUGH GRADING AND RE-SPREADING TOPSOIL IN ALL TURF AND LANDSCAPE AREAS (BEDS AND ISLANDS).
- THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR FINE GRADING AND PREPARATION OF ALL LAWN AND LANDSCAPE AREAS.
- REMOVE ALL EXISTING VEGETATION DURING GRADING PROCESS.
- APPLY MINIMUM OF SIX (6) INCHES OF CLEAN TOPSOIL(IMPORTED OR SCREEN ON -SITE) AND FINE GRADE. LEAVING TOPSOIL IN A LOOSE AND FRIABLE CONDITION FOR SEEDING.
- LIME SOIL OR ADD OTHER ORGANIC AMENDMENTS AS NECESSARY TO ACHIEVE A SOIL pH BETWEEN 5.5 - 7.0.
- LANDSCAPE CONTRACTOR SHALL WORK OVER LAWN AREAS THAT HAVE REMAINED PARTIALLY INTACT, TOP DRESSING WITH SOIL, SCARIFYING, AND SEEDING TO FORM A SMOOTH, FULL, EVEN LAWN, FREE OF BARE SPOTS, INDENTATIONS, AND WEEDS.
- SEEDING SHOULD BEGIN IMMEDIATELY UPON COMPLETION OF FINE GRADING. SEED SHOULD BE PRESSED INTO THE SOIL TO CREATE GOOD SEED-TO-SOIL CONTACT, NO DEEPER THAN THE THICKNESS OF THE SEED.
- FERTILIZING, APPLY 10-0-10 FERTILIZER EVENLY AT THE RATE OF 20 POUNDS PER 1000 SQ FT. NO FERTILIZER CONTAINING PHOSPHORUS IS PERMITTED ON SITE.
- SEED SHOULD BE APPLIED EITHER BY HAND BROADCASTING OR HYDRO SEEDING. TWO PASSES SHALL BE MADE IN PERPENDICULAR DIRECTIONS TO INSURE PROPER COVERAGE.
- LAWN SEED MIX

MIX A: SEEDING RATE: 6 LBS./1,000 SQ.FT  
LOW MAINTENANCE FESCUE LAWN  
PREFERRED SEED : LOW MAINTENANCE GRASS SEED MIX OR APPROVED EQUAL  
25% FIRELY HARD FESCUE  
25% BIG HORN GT HARD/SHEEP  
20% INTRIGUE CHEWINGS FESCUE  
20% QUATRO SHEEP FESCUE  
10% MINOTAUR HARD FESCUE

MIX B: SEEDING RATE: 4LBS./1,000 SQ.FT  
OCCASIONAL WET - WET LOCATIONS:  
20% RED TOP  
20% ALKALI GRASS  
10% AUTUMN BENTGRASS  
20% VIRGINIA WILD RYEGRASS  
20% FOX SEDGE  
10% FOWL BLUEGRASS

- DRY APPLICATION MULCH  
A. STRAW MULCH SHOULD BE APPLIED TO NEWLY SEEDED AREAS WITHIN 12 HOURS IF HYDRO MULCH IS NOT UTILIZED.
- DRY APPLICATION, STRAW: STALKS OF OATS, WHEAT, RYE OR OTHER APPROVED CROPS WHICH ARE FREE OF NOXIOUS WEEDS. WEIGHT SHALL BE BASED ON A 15 PERCENT MOISTURE CONTENT.
- DRY APPLICATION: WITHIN ONE DAY AFTER SEEDING, COVER THE SEEDED AREAS WITH A UNIFORM BLANKET OF STRAW MULCH AT THE RATE OF 100 POUNDS PER 1000 SQ FT OF SEEDED AREA.
- HYDRO APPLICATION: APPLY APPROVED MULCH IN ACCORDANCE WITH THE MANUFACTURER'S WRITTEN INSTRUCTIONS AND RECOMMENDED RATES OF APPLICATION. APPLY SEEDING MATERIALS WITH AN APPROVED HYDRO SEEDER.  
A. COLORED WOOD CELLULOSE FIBER PRODUCT SPECIFICALLY DESIGNED FOR USE AS A HYDRO-MECHANICAL APPLIED MULCH. ACCEPTABLE PRODUCT: CONWED HYDRO MULCH, CONWED FIBERS, 231 4TH STREET SW, HICKORY, NC
- DISTRIBUTE UNIFORMLY A SLURRY MIXTURE OF WATER, SEED, FERTILIZER, AND MULCH AT A MINIMUM RATE OF 57 GALLONS PER 1000 SQ FT (2500 GALLONS PER ACRE). THE OWNER AND PROJECT REPRESENTATIVE MAY ORDER THE AMOUNT OF WATER INCREASED IF DISTRIBUTION OF SEEDING MATERIALS IS NOT UNIFORM.

RED SUNSET MAPLE



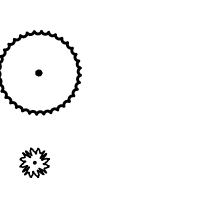
JAPANESE ZELKOVA



ROBIN HILL SERVICEBERRY



WHITE SPRUCE



COLORADO BLUE SPRUCE

EMERALD ARBORVITAE

NEON FLASH SPIREA

MISS KIM DWARF LILAC

DWARF MAIDEN GRASS

FOUNTAIN GRASS

RED TWIG DOGWOOD

YELLOW TWIG DOGWOOD

### PLANT SCHEDULE

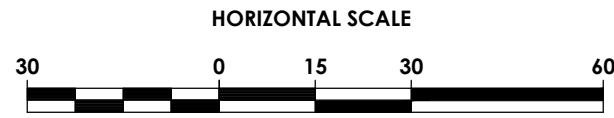
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	SPACING/SPREAD	REMARKS	MATURE HEIGHT	NATIVE	SALT TOLERANCE
DECIDUOUS TREES										
AR	3	ACER RUBRUM "RED SUNSET"	RED SUNSET MAPLE	2 1/2'-3'	B&B	AS SHOWN	FALL HAZARD	40-45'	YES	YES
ZS	1	ZELKOVA SERRATA "VILLAGE GREEN"	VILLAGE GREEN JAPANESE ZELKOVA	2 1/2'-3'	B&B	AS SHOWN	FALL HAZARD	50-65'		MODERATE
EVERGREEN TREES										
PG	5	PICEA GLAUCA	WHITE SPRUCE	6'-8'	B&B	10'-20'	30' O.C. BUFFER TREE	40-60'	YES	YES
PP	3	PICEA PUNGENS	COLORADO BLUE SPRUCE	6'-8'	B&B	10'-20'	30' O.C. BUFFER TREE	30-60'	YES	YES
TO	7	THUJA OCCIDENTALIS "EMERALD"	ARBORVITAE "EMERALD"	6'-8'	B&B	5' O.C.	DUMPSTER SCREEN	15-18'	YES	YES
FLOWERING AND ORNAMENTAL TREES										
AC	2	AMELANCHIER CANADENSIS "ROBIN HILL PINK"	ROBIN HILL SERVICEBERRY	2-2 1/2'	B&B	25'	TREE FORM	20'	YES	YES
DECIDUOUS SHRUBS										
SJ	25	SPIRAEA JAPONICA "NEON FLASH"	NEON FLASH SPIREA	24" H.	B&B	3' O.C.		2 1/2'-3'		
SP	17	SYRINGA PATULA "MISS KIM"	MISS KIM DWARF LILAC	30-36" H.	#5 CONT.	6' O.C.		5-6'		
GRASSES AND PERENNIALS										
MS	25	MISCANTHUS SINENSIS "ADAGIO"	DWARF MAIDEN GRASS	24" H.	#2 CONT.	4' O.C.		3-4'		
PA	12	PENNISETUM ALOPECUROIDES "HAEMEL"	FOUNTAIN GRASS	24" H.	#2 CONT.	3' O.C.		1 1/2'-2 1/2'		
SHRUBS										
CS	13	CORNUS SERICEA	RED TWIG DOGWOOD	24" H.	#3 CONT.	5' O.C.	STORM BASIN PLANTING	5-6'	YES	MODERATE
CSF	13	CORNUS SERICEA "FLAVIRAMEA"	YELLOW TWIG DOGWOOD	24" H.	#3 CONT.	5' O.C.	STORM BASIN PLANTING	5-6'	YES	MODERATE
NOTE: ROCK MULCH SHALL BE 1-3" WASHED RIVER ROCK, UNIFORM IN SIZE AND DEVOID OF ALL FINES MATERIAL SHALL BE FREE OF ORGANIC AND INORGANIC DEBRIS OR TRASH										

### LANDSCAPING REQUIREMENTS

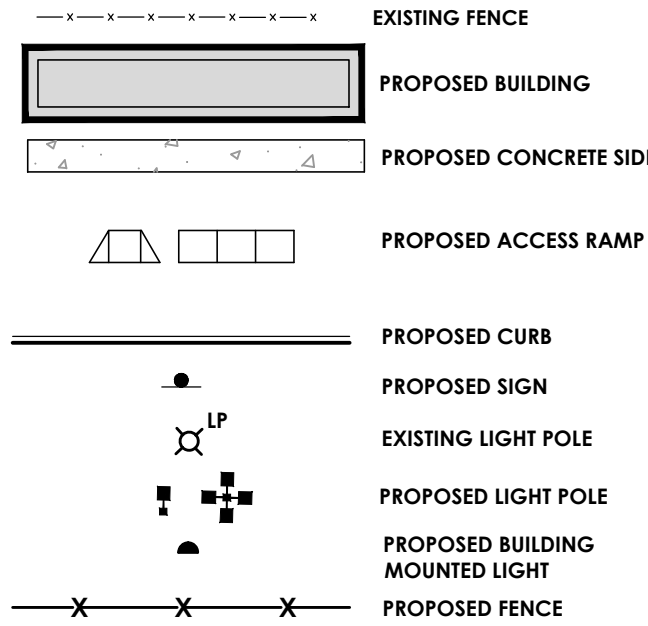
	REQUIRED	PROPOSED
PARING INTERNAL GREENSPACE	FOR 10-25 SPACES, 8% MINIMUM.	9.5%
	12,240SF OF PARKING AREA * 8% = 979SF OF PARKING LOT GREENSPACE	1,164SF OF GREENSPACE

### LANDSCAPING NOTES:

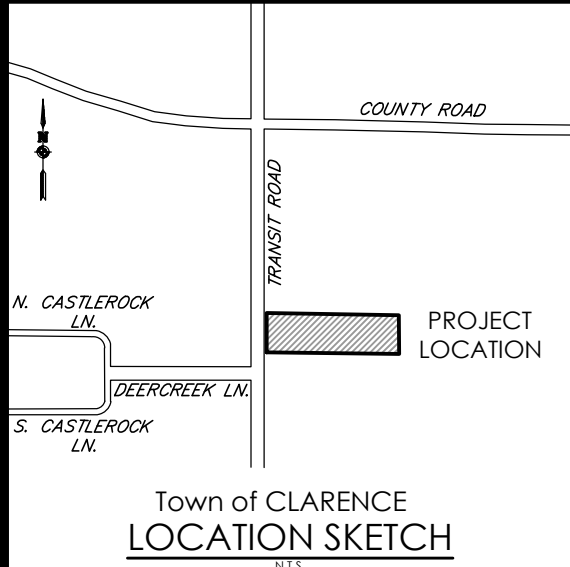
- CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND LOCAL PERMITS REQUIRED. ALL CONSTRUCTION SHALL CONFORM TO APPLICABLE TOWN AND STATE DESIGN STANDARDS AND CODES.
- IT IS THE LANDSCAPE CONTRACTORS RESPONSIBILITY TO VISIT THE SITE PRIOR TO BID SUBMITTAL, TO BECOME FAMILIAR WITH EXISTING CONDITIONS AT THE SITE.
- STANDARDS SET FORTH IN THE "AMERICAN STANDARD FOR NURSERY STOCK", ANSI Z60.1 (LATEST EDITION) REPRESENT GUIDELINE SPECIFICATIONS ONLY AND SHALL CONSTITUTE THE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS DELIVERED AND INSTALLED ON THIS PROJECT.
- ALL PLANTS MUST BE HEALTHY, VIGOROUS AND FREE OF PESTS AND DISEASE.
- ALL PLANTS MUST BE HARDY UNDER CLIMATE CONDITIONS THAT EXIST AT THE PROJECT SITE AND GROWN AT A NURSERY IN THE SAME HARDINESS ZONE AS THE PROJECT LOCATION.
- ALL PLANTS MUST BE CONTAINER GROWN OR BALLED AND BURLAPPED AN MEET SIZE REQUIREMENTS AS INDICATED ON THE PLANT LIST.
- ALL TREES MUST BE STRAIGHT-TRUNKED, INJURY FREE, HAVE A FULL, SYMMETRICAL CROWN (HEAD) AND MEET ALL REQUIREMENTS SPECIFIED (E.G. SINGLE STEM, MULTI-STEM, HEAVY BRANCHED, ETC.).
- ANY PROPOSED DEVIATION TO THE LANDSCAPE PLAN MUST FIRST BE REVIEWED AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO THE INSTALLATION OF THE PROPOSED LANDSCAPING CHANGES.
- THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL QUANTITIES SHOWN ON THESE PLANS. THE BID PRICE SUBMITTED WILL ASSUME THAT ALL PLANT MATERIALS DELINEATED WILL BE SUPPLIED AND INSTALLED. ANY DISCREPANCIES IN THE QUANTITIES SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER AND/OR DESIGN LANDSCAPE ARCHITECT (OWNER'S REPRESENTATIVE) PRIOR TO COMPLETING A BID PRICE.
- ALL GRADING AND UTILITY WORK SHALL BE COMPLETED PRIOR TO INSTALLATION OF PLANT MATERIAL AND LANDSCAPE MULCH.
- THE FINAL LOCATION OF TREES AND OTHER LANDSCAPING SHALL BE DETERMINED IN THE FIELD BASED ON UTILITY STAKEOUT AND SHALL NOT CONFLICT WITH TRAFFIC SIGNS AND/OR UTILITIES. STAKE OUT SHALL BE APPROVED BY OWNER'S REPRESENTATIVE PRIOR TO BEGINNING WORK.
- ANY CONCERNS RELATED TO SITE CONDITIONS AND/OR PLANT LOCATIONS SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION.
- PLANTING BACKFILL MIXTURE: 4 PARTS TOPSOIL (ON-SITE OR IMPORTED), 1 PART PEAT MOSS, 1/2 PART WELL ROTTED MANURE AND 10 LBS. 5-0-5 PLANTING FERTILIZER, MIXED THOROUGHLY PER CUBIC YARD.
- MULCH ALL PLANT BEDS, AND INDIVIDUAL TREES IN LAWN AREAS WITH SHREDDED HARDWOOD BARK MULCH TO A DEPTH OF THREE (3) INCHES UNLESS OTHERWISE SPECIFIED ON PLANTING DETAILS, OR AS DIRECTED BY THE LANDSCAPE ARCHITECT DUE TO SITE CONDITIONS.
- ANY PLANT WHICH TURNS BROWN, DEFOLIATES OR DIES PRIOR TO FINAL ACCEPTANCE BY THE OWNER, OR DESIGN LANDSCAPE ARCHITECT, SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH THE SAME PLANT (SPECIES, VARIETY AND SIZE) AS SPECIFIED ON THE PLANT SCHEDULE (LIST).
- THE CONTRACTOR SHALL MAINTAIN ALL PLANT MATERIALS AND LAWN AREAS UNTIL THE PROJECT HAS RECEIVED FINAL ACCEPTANCE BY THE OWNER OR OWNER'S REPRESENTATIVE. MAINTENANCE SHALL INCLUDE, BUT NOT BE LIMITED TO: WATERING, MULCHING, FERTILIZING, SPRAYING (FUNGICIDE, PESTICIDE, ANTI-DESICCANT), AS WELL AS RAISING PLANTS THAT HAVE SETTLED TOO DEEP OR REQUIRE STRAIGHTENING.
- UPON COMPLETION AND ACCEPTANCE OF THE LANDSCAPING, THE LANDSCAPE MATERIALS SHALL BE GUARANTEED FOR TWO (2) YEARS. THE GUARANTEE SHALL BE INCLUSIVE OF ALL MATERIAL AND LABOR COSTS. AT THE END OF THE GUARANTEE PERIOD THE OWNERS REPRESENTATIVE WILL INSPECT ALL PLANT MATERIALS. THE CONTRACTOR SHALL PROMPTLY MAKE ALL REQUIRED REPLACEMENTS WITH PLANT MATERIALS MEETING THE SPECIFICATIONS (E.G. SPECIES, SIZE AND CHARACTER).
- ALL AREAS DISTURBED BY SITE GRADING AND/OR UTILITY INSTALLATION SHALL RECEIVE APPROVED TOPSOIL (BASED ON APPROVED SAMPLES SUBMITTED BY THE CONTRACTOR) AND SPREAD TO A DEPTH NOT LESS THAN SIX (6) INCHES AFTER COMPACTION. TOPSOIL PLACED FOR LAWNS SHALL BE FINE GRADED, SEEDED, MULCHED AND WATERED UNTIL A HEALTHY STAND OF GRASS IS ESTABLISHED. THIS IS EXCLUDING FOUNDATION PLANT BEDS, AND ENTRANCE AREAS.
- LOCATIONS OF EXISTING BURIED UTILITIES SHOWN ON THE SITE PLAN ARE BASED UPON THE BEST AVAILABLE INFORMATION AND ARE TO BE CONSIDERED APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE TO CALL FOR A UTILITY STAKEOUT PRIOR TO COMMENCING PLANT INSTALLATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY AND ALL DAMAGE TO UTILITIES, STRUCTURES, AND SITE APPURTENANCES WHICH OCCURS AS A RESULT OF LANDSCAPE INSTALLATION OPERATIONS.
- EXISTING TREES INDICATED TO BE REMOVED SHALL OCCUR UNDER THE SITE CONTRACT FOR THIS PROJECT. THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR NEW PLANTINGS OR RESTORATION OF THE DISTURBED AREA (LAWNS, PLANT BEDS, ISLANDS).
- PRE-EMERGENT HERBICIDE SHALL BE USED UNDER MULCH IN ALL TREE AND PLANT BED AREAS.
- ALL SHRUB BEDS ADJACENT TO LAWN AREAS SHALL HAVE A SPADED EDGE BORDER, UNLESS METAL EDGE, CONCRETE, OR OTHER BORDER IS SPECIFIED.



### LEGEND - LANDSCAPING:



**PA**  
PASSERO ASSOCIATES  
engineering architecture



Client:  
**DANIELE FAMILY COMPANIES**  
2851 MONROE AVENUE  
ROCHESTER, NY 14618

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### Revisions

No.	Date	By	Description
1	11/01/21	ABG	PER TCWA COMMENT
2	01/24/22	ABG	PER TOWN COMMENT
3	04/14/22	ABG	PER NYSDOT COMMENTS
4	10/31/22	ABG	FULL ACCESS DRIVEWAY PER NYSDOT
5	12/05/22	ABG	PER TOWN ENGINEER COMMENTS

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### LIGHTING / LANDSCAPING PLAN RWD CLARENCE

Town/City: CLARENCE  
County: ERIE  
State: NEW YORK

Project No:  
**20213204.0001**

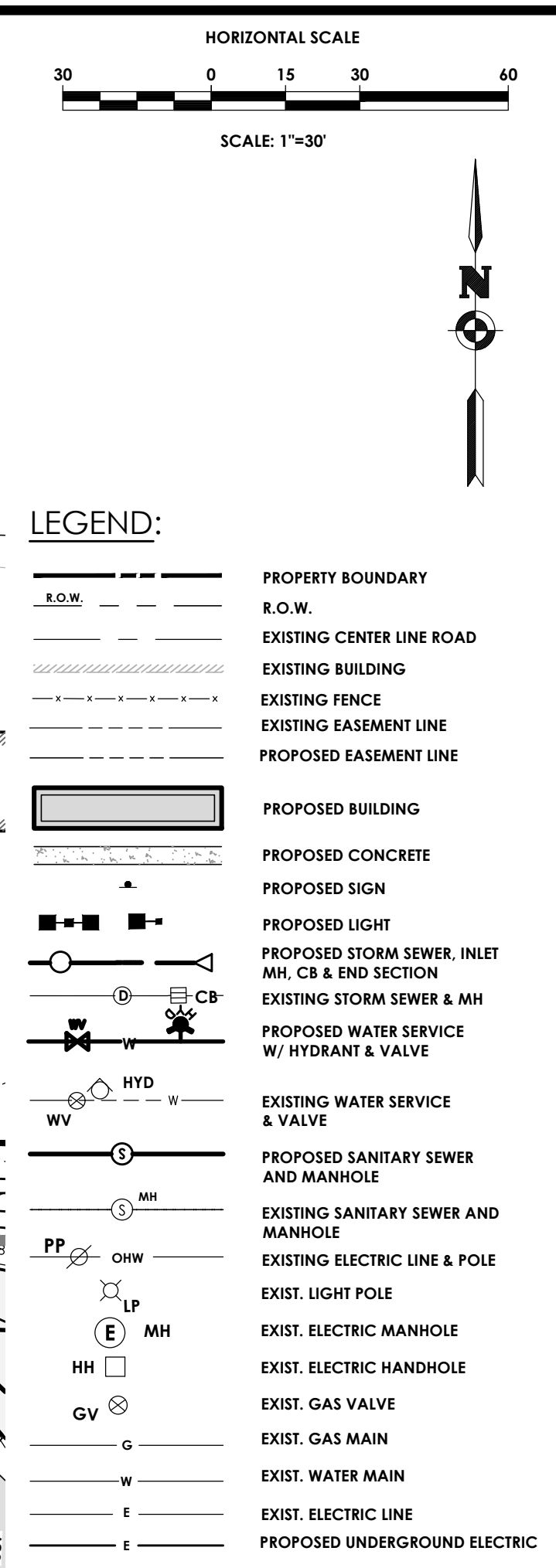
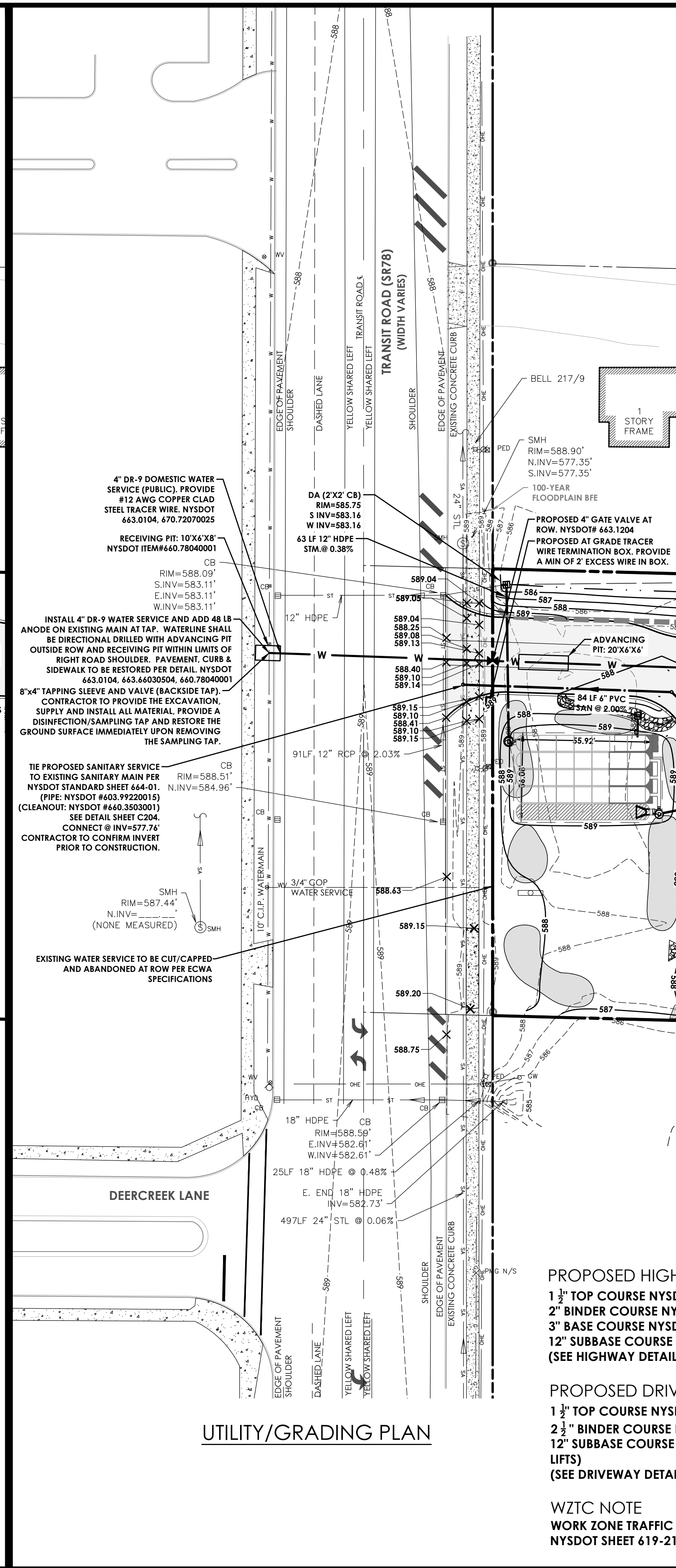
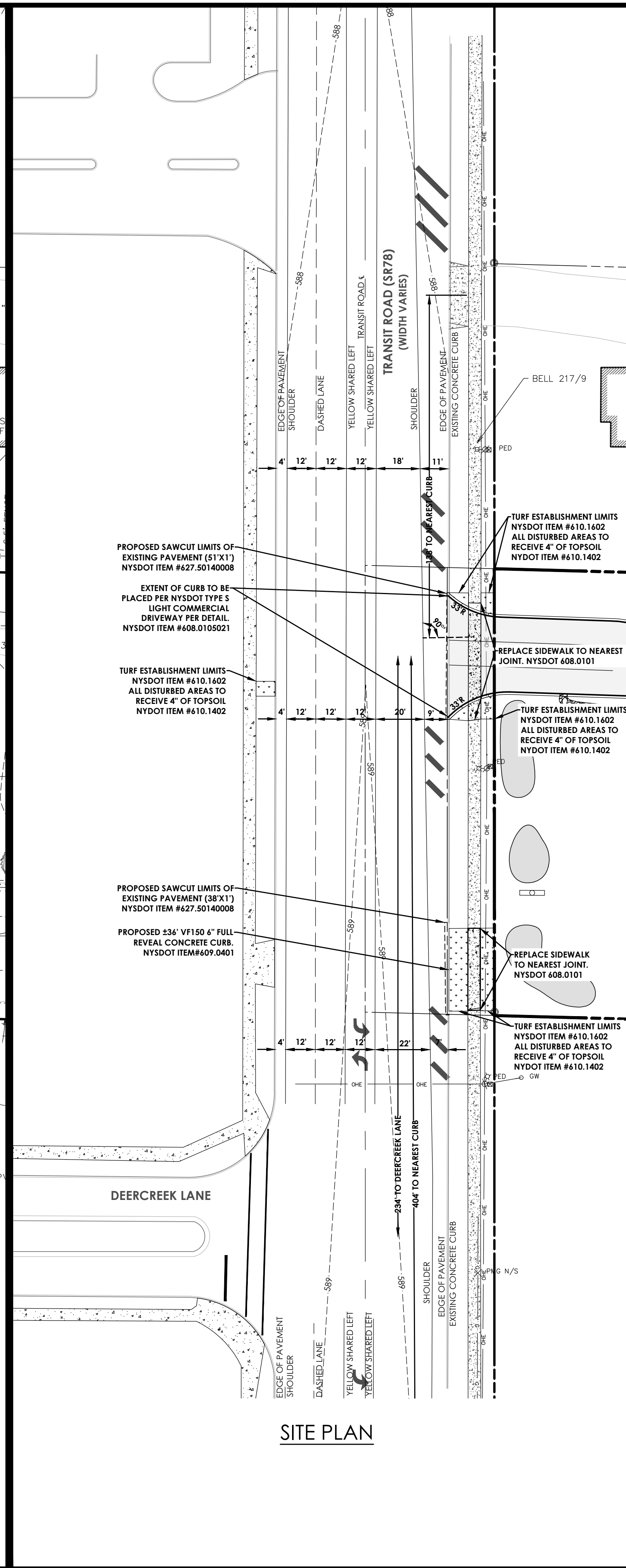
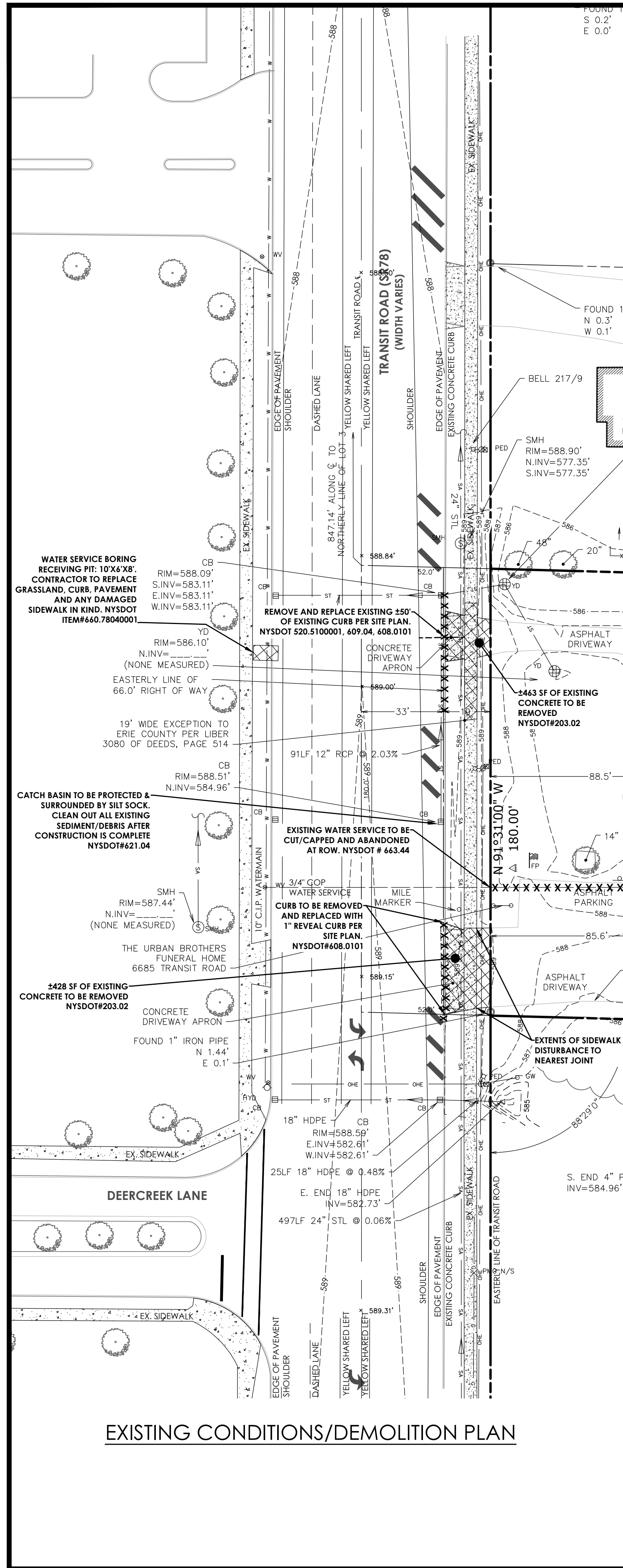
Drawing No. Sheet No.  
**C 107 7**

Scale:  
**1" = 30'**

Date  
**SEPTEMBER 2021**

NOT FOR CONSTRUCTION







CONSTRUCTION SEQUENCE FOR GRADING AND EROSION CONTROL:

1. INSTALL PERIMETER SILT FENCE.
2. INSTALL STABILIZED CONSTRUCTION ENTRANCE.
3. INSTALL POND AREAS.
4. CONSTRUCT DRAINAGE SWALES ALONG PROPERTY LINES AS SHOWN.
5. CLEAR AND GRUB THE PROJECT IMPROVEMENTS AREAS.
6. STRIP TOPSOIL AND TRUCK EXCESS OFF-SITE.
7. GRADE IMPROVEMENTS AREAS WITHIN THE PROJECT SITE. AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR MORE THAN 7 DAYS WILL BE STABILIZED WITH A TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST DISTURBANCE.
8. CONSTRUCT DROP INLET PROTECTION DEVICES AFTER STORM SEWER INSTALLATION AS SHOWN ON THIS PLAN.
9. REPLACE TOPSOIL AND FINE GRADE.
10. HYDRO-SEED ALL DISTURBED AREAS WITHIN 7 DAYS AFTER FINAL GRADING, CONTRACTOR IS RESPONSIBLE TO RESEED IF GRADING IS UNSATISFACTORY.
11. UPON APPROVAL OF THE TOWN, REMOVE ALL TEMPORARY SILTATION CONTROLS.
12. SLOPES SHALL NOT EXCEED 1" VERTICAL TO 3" HORIZONTAL MAX. MAINTAIN 1:4 WHERE POSSIBLE.
13. MINIMUM OF 6" OF TOPSOIL IS TO BE PLACED ON ALL GRASS AREAS.
14. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BASED UPON ACTUAL FIELD CONDITIONS AOB. CONTRACTOR SHALL PROVIDE FOR THIS COST IN HIS CONTRACT.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SITUATION AND EROSION CONTROL MEASURES FROM INSTALLATION THROUGH MAINTENANCE AND REMOVAL AFTER REVEGETATION HAS BEEN ESTABLISHED.
16. ALL END SECTIONS WILL BE PROVIDED WITH RIP-RAP APRONS.
17. ALL EROSION AND SEDIMENT CONTROL METHODS WILL BE DESIGNED AND INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.
18. ALL EROSION CONTROL MEASURES WILL BE REMOVED AFTER APPROVED BY QUALIFIED PROFESSIONAL.

AT THE VERY MINIMUM, EROSION CONTROL SHALL BE AS SHOWN ON THIS PLAN. EROSION CONTROL MAY CONSIST OF SEDIMENT TRAPS AND/OR ENVIRONMENTAL FENCES. THE CONTRACTOR AND THE DEVELOPER SHALL BE RESPONSIBLE FOR THE INTEGRITY, MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES UNTIL NO LONGER DEEMED NECESSARY BY THE TOWN OR IT'S REPRESENTATIVE. THE CONTRACTOR SHALL MAINTAIN THE STORM SEWER SYSTEM UNTIL THE PROJECT IS DEVELOPED AND APPROVED BY THE TOWN AND OWNER.

ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD WORKING ORDER. THE OWNER SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE STONE FILL. CORRECTIVE ACTIONS, AS IDENTIFIED BY THE DEVELOPER'S ONSITE REPRESENTATIVE OR A TOWN REPRESENTATIVE, SHALL BE INITIATED WITHIN 24 HOURS OF BEING REPORTED. THE TOWN MAY REVIEW THE PROJECT SITE AT ANY TIME. REVIEW OF EROSION CONTROL MEASURES BY THE TOWN DOES NOT RELIEVE THE DEVELOPER OF HIS OBLIGATIONS TO EXECUTE RESPONSIBLE STANDARD MANAGEMENT PRACTICES

PUBLIC WATER SERVICE LINE NOTES (4" & GREATER):

1. WATER SERVICE LINES (LATERALS) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REGULATION AND SPECIFICATIONS OF THE ERIE COUNTY WATER AUTHORITY.
2. WATER SERVICE LINES SHALL HAVE A MINIMUM OF FIVE FEET OF COVER FROM FINISHED GRADE IN LAWN AREAS AND SIX FEET COVER FROM FINISHED GRADE IN PAVED AREAS.
3. WATER SERVICE LINES SHALL BE SEPARATED AT LEAST TEN FEET, MEASURED FROM THE OUTSIDE OF THE PIPES, FROM SEWER MAINS OR SEPTIC SYSTEMS.
4. WATER SERVICE LINES SHALL BE AS IDENTIFIED AS:

	SIZE	MATERIAL (C)	TYPE (D)
ROYAL CAR WASH			
ECWA PORTION = FROM THE WATER MAIN TO AND INCLUDING THE CONTROL VALVE ON THE ROW/PROPERTY/EASEMENT LINE	6"	DR-9	CMB
PRIVATE PORTION = FROM THE CONTROL VALVE TO THE METER	6"	DR-9	CMB

- (b) SERVICE TYPES INCLUDE: DOMESTIC = DS, FIRE = FS, OR COMBINED = CMB
5. THE ECWA PORTION OF THE WATER SERVICE LINE SHALL BE INSTALLED PRIOR TO THE PRIVATE PORTION OF THE SERVICE LINE.
  6. WATER METER(S) TO BE LOCATED ON THE INTERIOR OF EXTERIOR WALL(S) IMMEDIATELY UPON SERVICE ENTRANCE INTO THE BUILDING(S). RESIDENTIAL SERVICES: A BY-PASS ASSEMBLY IS NOT REQUIRED AROUND THE INSTALLATION OF 1/2 INCH THROUGH 2-INCH METERS. NON-RESIDENTIAL SERVICES: THE INSTALLATION OF AN 1/2 INCH METER OR LARGER REQUIRES A BY-PASS ASSEMBLY AROUND THE METER.
  7. WATER SERVICE LINES SIZED 4-INCHES OR GREATER SHALL BE:
    - \* PRESSURE TESTED IN ACCORDANCE WITH THE LATEST SPECIFICATIONS OF THE ERIE COUNTY WATER AUTHORITY. A WATER AUTHORITY REPRESENTATIVE MUST WITNESS THIS TEST.
    - \* DISINFECTED BY USING THE CONTINUOUS FEED METHOD ACCORDING TO AWWA STANDARD SPECIFICATIONS. AFTER FLUSHING AND DISINFECTING THE SERVICE LINE, WATER SAMPLES SHALL BE COLLECTED BY THE NYS DEPARTMENT OF HEALTH. APPROVAL AND NOTIFICATION BY THE HEALTH DEPARTMENT MUST BE RECEIVED BEFORE THE LATERAL IS PLACED IN SERVICE.

STANDARD WATER MAIN EXTENSION NOTES:

1. THE WATER MAIN SHALL BE DISINFECTED EQUAL TO AWWA STANDARD FOR DISINFECTING WATER MAINS DESIGNATION C651 (LATEST REVISION), FOLLOWING DISINFECTION, THE WATER MAIN PIPELINE SHALL BE FLUSHED UNTIL THE CHLORINE CONCENTRATION IN THE WATER LEAVING THE MAIN IS NO HIGHER THAN THAT GENERALLY PREVAILING IN THE SYSTEM. ALL WATER MAIN FITTINGS NOT RECEIVING 24-HOUR CHLORINE DISINFECTION CONTACT TIME MUST BE SWAB DISINFECTED 30 MINUTES PRIOR TO INSTALLATION. THE SAMPLING POINT(S) MUST BE DECONTAMINATED BY FLAMING. FIRE HYDRANTS ARE NOT ACCEPTABLE SAMPLING POINTS. THE NYS DEPARTMENT OF PUBLIC HEALTH MUST RECEIVE AT LEAST 48-HOUR ADVANCE NOTIFICATION REQUESTING SAMPLING SERVICES. SAMPLING WILL NOT BE PERFORMED PRIOR TO RECEIPT FROM A NEW YORK STATE LICENSED OR REGISTERED DESIGN PROFESSIONAL (ENGINEER, ARCHITECT OR LAND SURVEYOR WITH A SPECIAL EXEMPTION UNDER SECTION 7208(n) OF THE EDUCATION LAW) CERTIFYING THAT THE WATER SUPPLY IMPROVEMENTS, TESTING AND DISINFECTION PROCEDURES WERE COMPLETED IN ACCORDANCE WITH THE APPROVED PLANS, REPORTS, SPECIFICATIONS AND ANY APPROVED AMENDMENTS. THE DEPARTMENT WILL COLLECT SAMPLES FOR FREE CHLORINE RESIDUAL, TOTAL COLIFORM, ESCHERICHIA COLI (E. COLI) AND TURBIDITY. THE WATER MAIN PIPE AND APPURTENANCES SHALL NOT BE PLACED INTO SERVICE UNTIL SO AUTHORIZED BY THE ERIE COUNTY DEPARTMENT OF PUBLIC HEALTH.
2. MINIMUM VERTICAL SEPARATION BETWEEN WATER MAIN PIPELINES AND SEWER PIPELINES SHALL BE 18 INCHES MEASURED FROM THE OUTSIDE OF THE PIPE AT THE POINT OF CROSSING. ONE FULL STANDARD LAYING LENGTH OF WATER MAIN SHALL BE CENTERED UNDER OR OVER THE SEWER SO THAT BOTH JOINTS WILL BE AS FAR FROM THE SEWER AS POSSIBLE. IN ADDITION, WHEN THE WATER MAIN PASSES UNDER A SEWER, ADEQUATE STRUCTURAL SUPPORT (COMPACTED SELECTED FILL) SHALL BE PROVIDED FOR THE SEWER TO PREVENT EXCESSIVE DEFLECTION OF JOINTS AND SETTLING OF THE SEWER ON THE WATER MAIN. MINIMUM HORIZONTAL SEPARATION BETWEEN PARALLEL WATER MAINS AND SEWER PIPES (INCLUDING MANHOLES AND VAULTS) SHALL BE 10 FEET MEASURED FROM THE OUTSIDE OF THE PIPES, MANHOLES OR VAULTS.
3. WHEN INSTALLING FIRE HYDRANTS, SHOULD GROUND WATER BE ENCOUNTERED WITHIN SEVEN (7) FEET OF THE FINISHED GRADE, FIRE HYDRANT WEEP HOLES (DRAINS) SHALL BE PLUGGED.
4. THE WATER MAIN PIPELINE AND APPURTENANCES SHALL BE PRESSURE/LEAKAGE TESTED IN ACCORDANCE WITH THE MINIMUM REQUIREMENTS OF THE AWWA STANDARD C600 (LATEST REVISION) OR IN ACCORDANCE WITH MORE STRINGENT REQUIREMENTS IMPOSED BY THE SUPPLIER OF WATER.

UTILITY NOTES:

1. PRIOR TO THE START OF UTILITY INSTALLATION THE CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR COORDINATION OF ALL UTILITY CONNECTIONS WITH MECHANICAL/ARCHITECTURAL DRAWINGS FOR INCLUDING BUT NOT LIMITED TO VERTICAL AND HORIZONTAL LOCATION, PENETRATIONS, AND SIZES. THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION TO PROCEED WITH UTILITY INSTALLATION BY THE OWNERS ONSITE REPRESENTATIVE UPON COMPLETION OF COORDINATION WITH CONTRACTORS, AND PLANS.
2. THE DEVELOPER AND HIS/HER CONTRACTOR IS RESPONSIBLE FOR COORDINATING GAS, ELECTRICAL, CABLE, TELEPHONE AND ANY OTHER UTILITIES NOT SPECIFICALLY SHOWN WITHIN THIS PLAN SET WITH APPROPRIATE AGENCY. PASSERO ASSOCIATES ASSUMES NO RESPONSIBILITY FOR THE DESIGN OR PERFORMANCE OF UTILITIES NOT SPECIFICALLY SHOWN WITHIN THIS PLAN SET.
3. PRIOR TO THE START OF UTILITY INSTALLATION THE CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES VERTICALLY AND HORIZONTALLY AND COORDINATE WITH EXISTING UTILITIES SHOWN ON THE PLANS AND REPORT ANY DISCREPANCIES TO THE DESIGN ENGINEER. THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION TO PROCEED WITH UTILITY INSTALLATION BY THE OWNERS ONSITE REPRESENTATIVE UPON COMPLETION EXISTING UTILITY VERIFICATION.
4. THRUST BLOCKS ON THE WATERMAIN ARE REQUIRED AT BENDS, TEES OR PLUGS. SEE DETAIL SHEETS FOR THRUST BLOCK DETAILS.

ELECTRIC & GAS SERVICE COORDINATION NOTES:

1. ELECTRIC AND PLUMBING CONTRACTORS SHALL COORDINATE WITH EACH OTHER AND DIRECTLY WITH NEW YORK STATE ELECTRIC & GAS CORPORATION (NYSEG) FOR METER AND SERVICE LOCATIONS PRIOR TO INSTALLATION.
2. ELECTRIC AND PLUMBING CONTRACTORS SHALL ALLOW FOR THE NECESSARY PROVISIONS WHEN INSTALLING METERS AND SERVICES IN ACCORDANCE WITH NYSEG DESIGN AND INSTALLATION GUIDELINES.
3. ELECTRIC AND PLUMBING CONTRACTORS SHALL DIRECTLY COORDINATE WITH EACH OTHER AND DIRECTLY WITH NYSEG FOR ELECTRIC AND GAS START-UP, RESPECTIVELY.

GENERAL NOTES

1. THE SANITARY WASTE DISPOSAL SYSTEM SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS OF THE NEW YORK STATE DEPARTMENT OF HEALTH AND THE TOWN OF CLARENCE.
2. WATER SERVICE WILL BE INSTALLED IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE ERIE COUNTY WATER AUTHORITY.
3. THE BUILDING CONSTRUCTION TO BE IN COMPLIANCE WITH THE NEW YORK STATE BUILDING CODE.
4. ANY COST RELATED TO THE RELOCATION OF ANY UTILITIES NECESSITATED BY THIS PROJECT SHALL BE THE RESPONSIBILITY OF THE OWNER OR THOSE REQUESTING THE RELOCATION OF THE UTILITY.
5. ANY AND ALL CULVERT LOCATIONS, SIZES OR TYPES, TO BE APPROVED BY THE TOWN SUPERINTENDENT OF HIGHWAYS.
6. ALL PROPERTY CORNERS TO BE MARKED WITH IRON PINS.
7. THIS SUBDIVISION SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF THE COUNTY MONUMENTATION LAW.
8. THE SUBDIVISION IS LESS THAN 1500 FEET FROM THE NEAREST CONTROL MONUMENT AND IS THEREFORE TIED INTO THE MONUMENTATION NETWORK.
9. THIS SUBDIVISION SHALL COMPLY WITH ANY COUNTY OR STATE AGENCIES HAVING JURISDICTION.
10. ALL SITE WORK TO BE IN COMPLIANCE WITH THE STANDARDS OF CHAPTER 29 - ARTICLE V OF THE TOWN OF CLARENCE.
11. ALL CONSTRUCTION WILL BE IN ACCORDANCE WITH THE DESIGN CRITERIA AND CONSTRUCTION SPECIFICATIONS FOR LAND DEVELOPMENT FOR THE TOWN OF CLARENCE DATED THE MOST CURRENT REVISION.

STORM NOTES

1. STORM SEWERS AND APPURTENANCES SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE LATEST REGULATIONS OF THE MUNICIPALITY
2. PROPOSED STORM SEWER LATERAL MATERIAL: PVC SDR-35 8" MIN. SIZE & SHALL BE LAID AT A MINIMUM GRADE OF 1/4" PER FT. STORM SEWER MATERIAL: HDPE 12" MIN.
3. FOUNDATION DRAINS SHALL BE CONNECTED TO STORM WATER SYSTEM VIA SUMP PUMPS. DOWNSPOUTS SHALL BE CONNECTED TO STORM SEWER WHERE APPLICABLE, WHERE NOTED ON THE PLANS DOWNSPOUTS SHALL DISCHARGE TO SPLASH BLOCKS.
4. UPON COMPLETION OF SYSTEM INSTALLATION, THE MAIN SEWER SYSTEM AND LEADS TO STRUCTURES SHALL BE FLUSHED AND LAMPED TO THE SATISFACTION OF THE MUNICIPALITY.

SANITARY NOTES

1. SANITARY SEWERS AND APPURTENANCES SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE LATEST REGULATIONS OF THE STATE, COUNTY AND LOCAL MUNICIPALITY.
2. MATERIALS
  - MAINS - PIPING SHALL BE POLYVINYL CHLORIDE (PVC) WITH ENDS SUITABLE FOR ELASTOMERIC GASKET JOINTS, AND A MINIMUM WALL THICKNESS OF SDR-35. PIPING AND FITTINGS SHALL MEET: ASTM D-3034 (4" THRU 15") ASTM F-479 (18" THRU 48")
  - LATERALS - 4" MIN. INSTALLED AT 1/4" PER FOOT MIN. PIPING SHALL BE POLYVINYL CHLORIDE (PVC) WITH ENDS SUITABLE FOR ELASTOMERIC GASKET JOINTS, AND A MINIMUM WALL THICKNESS OF SDR-21. PIPING AND FITTINGS SHALL MEET ASTM D-2241.
  - JOINTING MATERIALS - SHALL BE BELL-AND-SPIGOT WITH INTEGRAL PUSH ON TYPE ELASTOMERIC GASKET JOINTS, GASKET MATERIAL TO BE NEOPRENE MEETING ASTM D-3212.
  - MANHOLES - SHALL BE PRECAST CONCRETE WITH NEOPRENE GASKETS MEETING ASTM C-478 & ASTM C-443.
3. INFILTRATION AND EXFILTRATION FOR SANITARY SEWERS SHALL BE LIMITED TO 100 GALLONS PER MILE PER INCH DIAMETER OF PIPE PER 24 HOURS.
4. IF AN AIR TEST IS USED, THE TEST AS A MINIMUM SHALL CONFORM TO THE PROCEDURE DESCRIBED IN ASTM C-828-80, ENTITLED STANDARD PRACTICE FOR LOW PRESSURE AIR TEST OF VITRIFIED CLAY PIPELINES; SANITARY MANHOLES SHALL BE TESTED FOR EXFILTRATION.
5. VACUUM TESTING OF MANHOLES IS ALLOWED. THE CONTRACTOR IS CAUTIONED TO SPEAK TO THE SUPERINTENDENT OF SEWERS PRIOR TO COMMENCING WITH PLANS TO VACUUM TEST.
6. DEFLECTION TEST - TEN STATE STANDARDS.
  - 6.A. DEFLECTION TESTS SHALL BE PERFORMED ON ALL FLEXIBLE PIPE. THE TEST SHALL BE CONDUCTED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS.
  - 6.B. IF THE DEFLECTION TEST IS TO BE RUN USING A RIGID BALL OR MANDRELL, IT SHALL HAVE A DIAMETER EQUAL TO 95% OF THE INSIDE DIAMETER OF THE PIPE. THE TEST SHALL BE PERFORMED WITHOUT MECHANICAL PULLING DEVICES.
- 6.C. NO PIPE SHALL EXCEED A DEFLECTION OF 5%.
7. ALL SANITARY SEWER INSTALLATION SHALL BE MADE IN CONFORMANCE WITH THE SPECIFICATIONS, REGULATIONS, AND POLICIES OF THE ERIE SEWER DISTRICT.
8. ALL HOUSE LATERALS SHALL HAVE A CLEANOUT AT THE PROPERTY LINE OR EASEMENT LINE.
9. FLOOR DRAINS, IF CONSTRUCTED, SHALL BE CONNECTED TO THE SANITARY SEWER/COMBINATION SEWER. (FLOOR DRAINS DO NOT INCLUDE FOUNDATION/FOOTER DRAINS). ALL DISCHARGES TO THE SANITARY/ COMBINATION SEWER MUST COMPLY WITH THE EFFLUENT LIMITS OF THE LOCAL AND/OR THE ERIE COUNTY SEWER USE LAW.
10. SEPARATION - MINIMUM VERTICAL SEPARATION BETWEEN WATER MAINS AND SEWER LINES SHALL BE 18 INCHES MEASURED FROM THE OUTSIDE OF THE PIPES AT THE POINT OF CROSSING. ONE FULL STANDARD LAYING LENGTH OF WATER MAIN SHALL BE CENTERED UNDER OR OVER THE SEWER SO THAT BOTH JOINTS WILL BE AS FAR FROM THE SEWER AS POSSIBLE. IN ADDITION, WHEN THE WATER MAIN PASSES UNDER A SEWER ADEQUATE STRUCTURAL SUPPORT (COMPACTED SELECTED FILL) SHALL BE PROVIDED FOR THE SEWER TO PREVENT EXCESSIVE DEFLECTION OF JOINTS AND SETTLING OF THE SEWER ON THE WATER MAIN. MINIMUM HORIZONTAL SEPARATION BETWEEN PARALLEL WATER MAINS AND SEWER PIPES (INCLUDING MANHOLES AND VAULTS) SHALL BE 10 FEET MEASURED FROM THE OUTSIDE OF THE PIPES, MANHOLES OR VAULTS.
11. SEWER USE LAW: FLOOR DRAINS, IF CONSTRUCTED, SHALL BE CONNECTED TO THE SANITARY/COMBINATION SEWER. FLOOR DRAINS DO NOT INCLUDE FOUNDATION/FOOTER DRAINS. NOTE: ALL DISCHARGES TO THE SANITARY/COMBINATION SEWER MUST COMPLY WITH THE EFFLUENT LIMITS OF THE LOCAL AND OR ERIE COUNTY SEWER USE LAW.



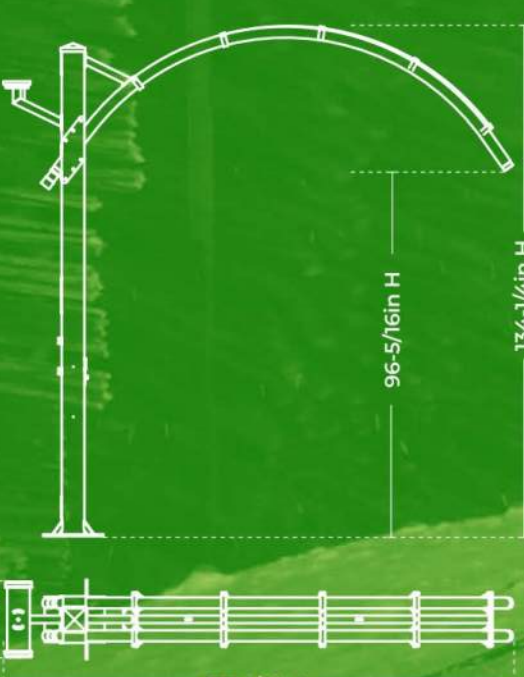
## Vacuum Booms



Provide customers with an inviting free vacuum area that doesn't require a lot of staff, or effort, to keep ready to serve the next customer.

The boom style keeps hoses securely off the ground providing better access to both sides of the vehicle and reducing wear and tear. Choose from different boom styles, canopy colors, drop kits, attachments and piping.

### Double-Drop Boom



### Single Drop Boom

Dimensions	114'-1/2inL x 20inW x 132'-7-8inH
Clearance	Height Clearance 95'-5/16inH



#### Custom Color Powder Coating Options

Contact a Sonny's Sales Representative for details



Join Sonny's  
**OneWash**  
and Save

### MAXIMIZE CUSTOMER EXPERIENCE



DESCRIPTION	PRICE
Single VAC Drop 2500 with Powder Coating	<b>\$1,834.00</b>
Vacuum Canopy Double Drop Boom with Powder Coating	<b>\$3,130.00</b>
Vacuum Canopy Boom with Powder Coating	<b>\$2,310.00</b>

\* No accessories included



#### Boom Options Available

The post mounted holster provides an air tight connection, reducing energy consumption and eliminating unnecessary air draw. Requires 1 Box of 1-1/2in or 2in hose for every 3 booms. For complete size, color selection and pricing refer to the vacuum parts section.

DROP KITS	CREVICE TOOL	EXPRESS CLAW
Single Vacuum Drop Kit Includes: (1) inlet valve (1) hose valve (1) cuff (1) swivel cuff (1) tool & holster selected. Vacuum hose not included	<b>\$106.46</b>	<b>\$117.03</b>
Dual Vacuum Drop Kit Includes: (1) double Y adapter (2) inlet valves (2) hose valves (2) cuffs (2) swivel cuffs (2) tools & holsters selected. Vacuum hose not included	<b>\$291.72</b>	<b>\$304.29</b>
1-1/2in Vacuum Hose (Box)	<b>Requires 1 box of hose for every 3 booms. See Vacuum Parts section for colors and pricing</b>	
2in Vacuum Hose (Box)		



#### Vacuum Post Foot Covers

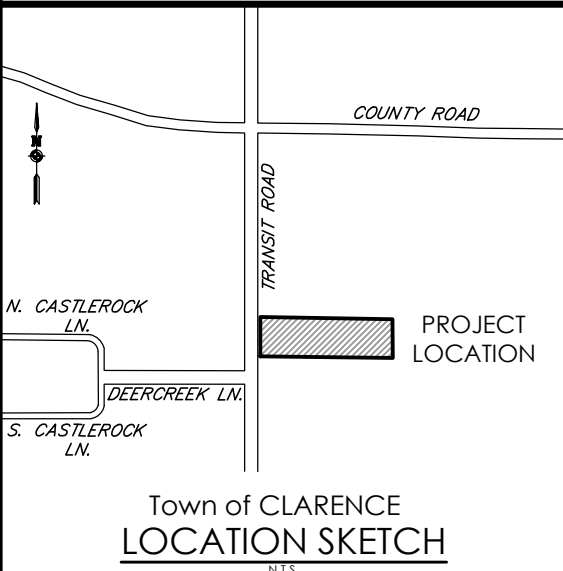
- Fits all vacuum posts and booms
- Attractive polypropylene cover
- Improves site appearance and reduces trip hazards

ITEM NO.	DESCRIPTION	PRICE
10012466	Vacuum Post, Foot Cover set w/hdwr Black	<b>\$54.05</b>
10012467	Vacuum Post, Foot Cover set w/hdwr Red	<b>\$55.11</b>
10012468	Vacuum Post, Foot Cover set w/hdwr Blue	<b>\$55.11</b>

**www.SonnysDirect.com** | 1-800-327-8723

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Revisions			
No.	Date	By	Description
1	11/01/21	ABG	PER ECWA COMMENT
2	01/24/22	ABG	PER TOWN COMMENT
3	04/14/22	ABG	PER NYSDOT COMMENTS
4	10/31/22	ABG	FULL ACCESS DRIVEWAY PER NYSDOT
5	12/05/22	ABG	PER TOWN ENGINEER COMMENTS

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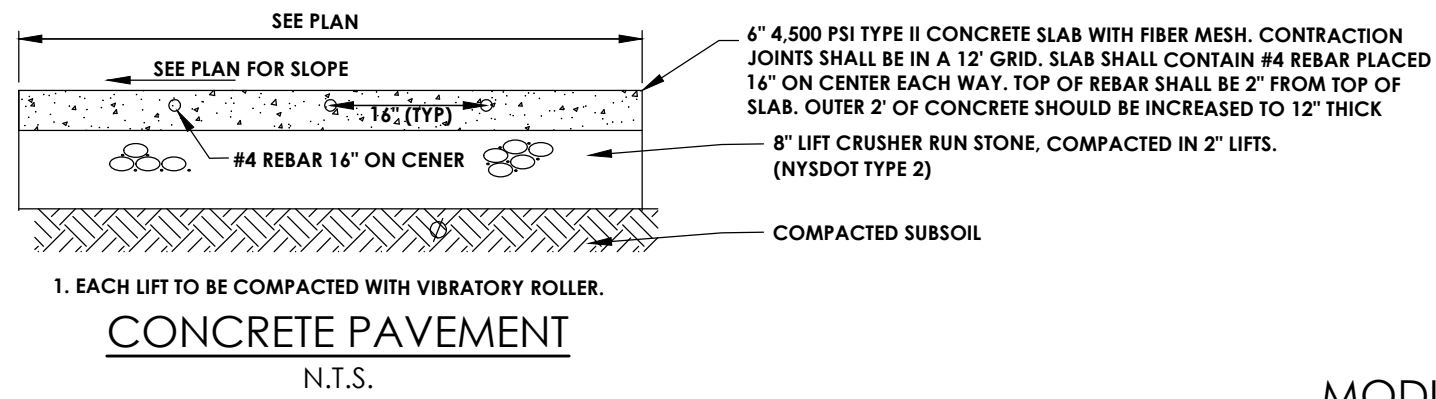
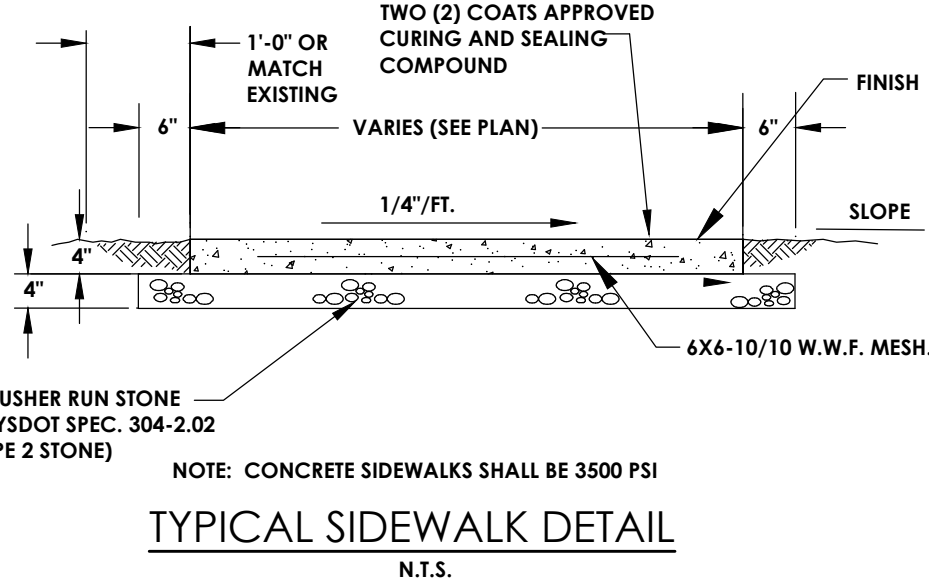
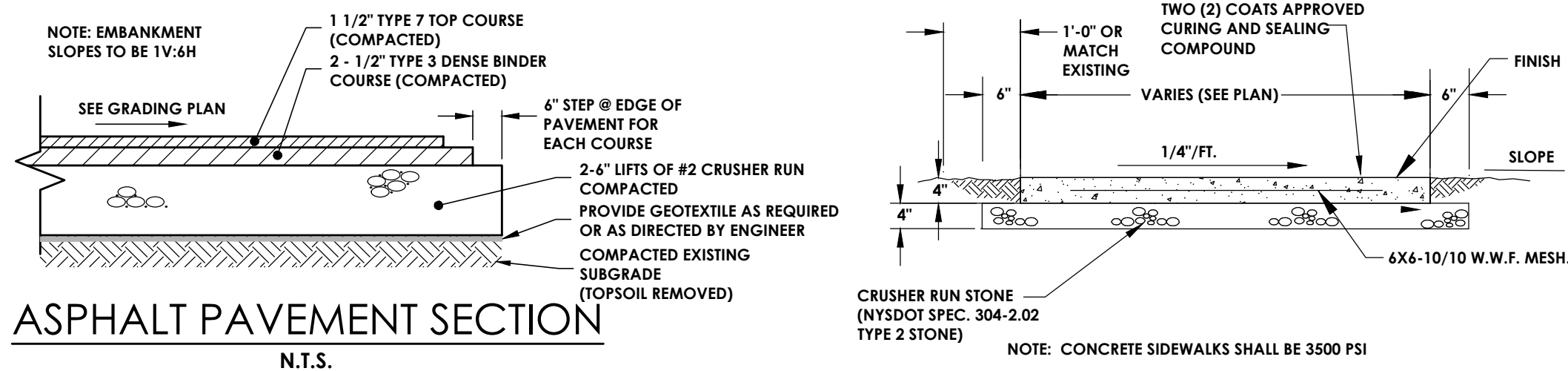
NOTES AND DETAILS

RWD CLARENCE

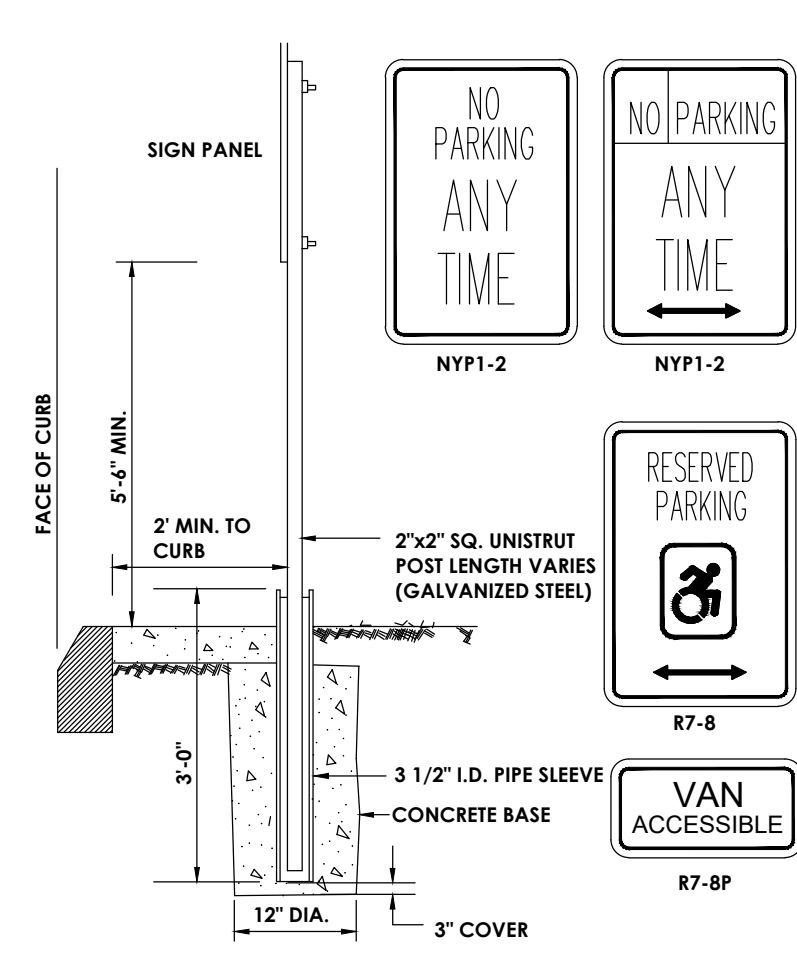
County: ERIE		Town/City: CLARENCE	
Project No.		State: NEW YORK	
20213204.0001			
Drawing No.	C 201	Sheet No.	9
Scale:	N.T.S.		
Date	SEPTEMBER 2021		

NOT FOR CONSTRUCTION

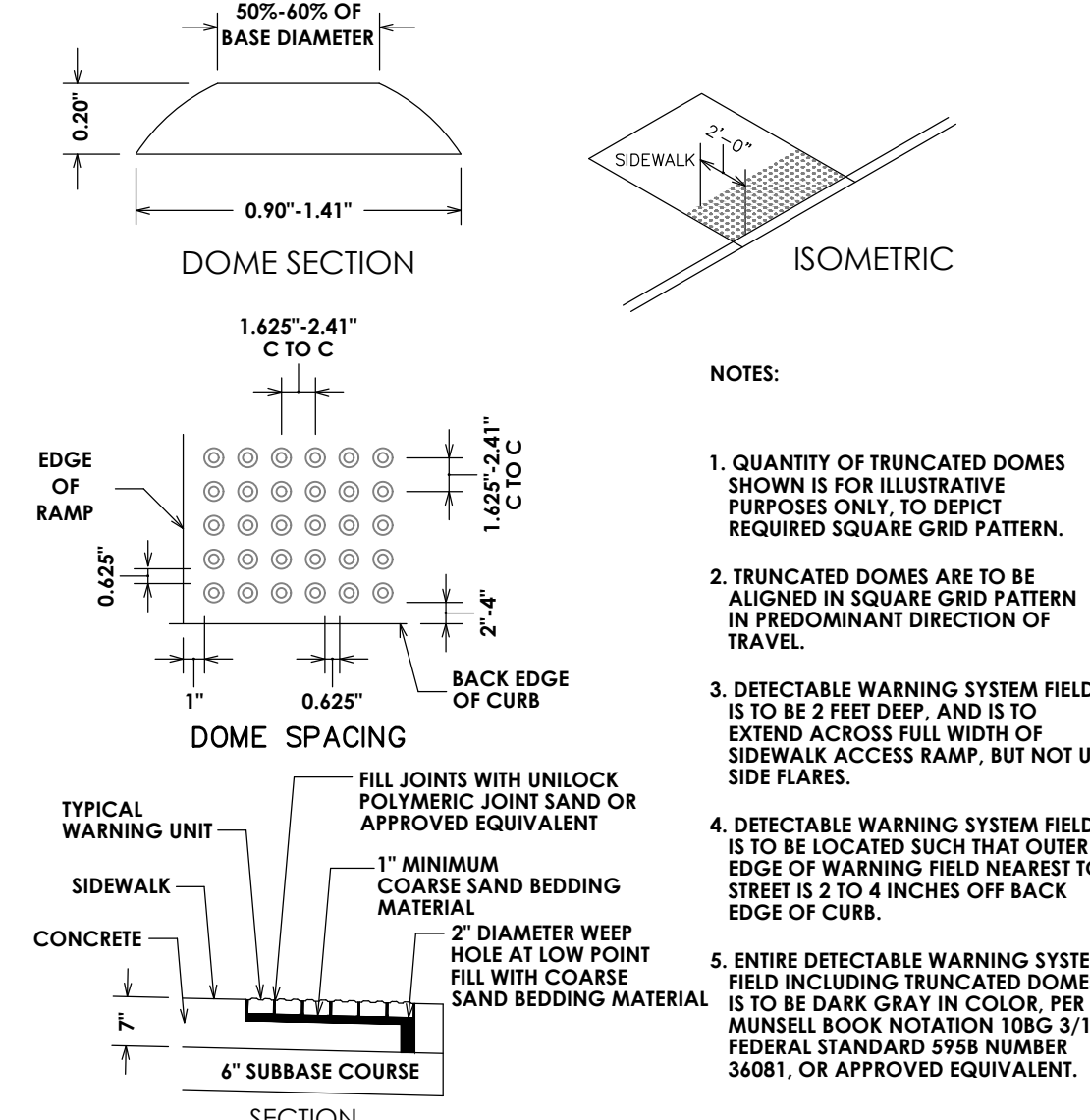




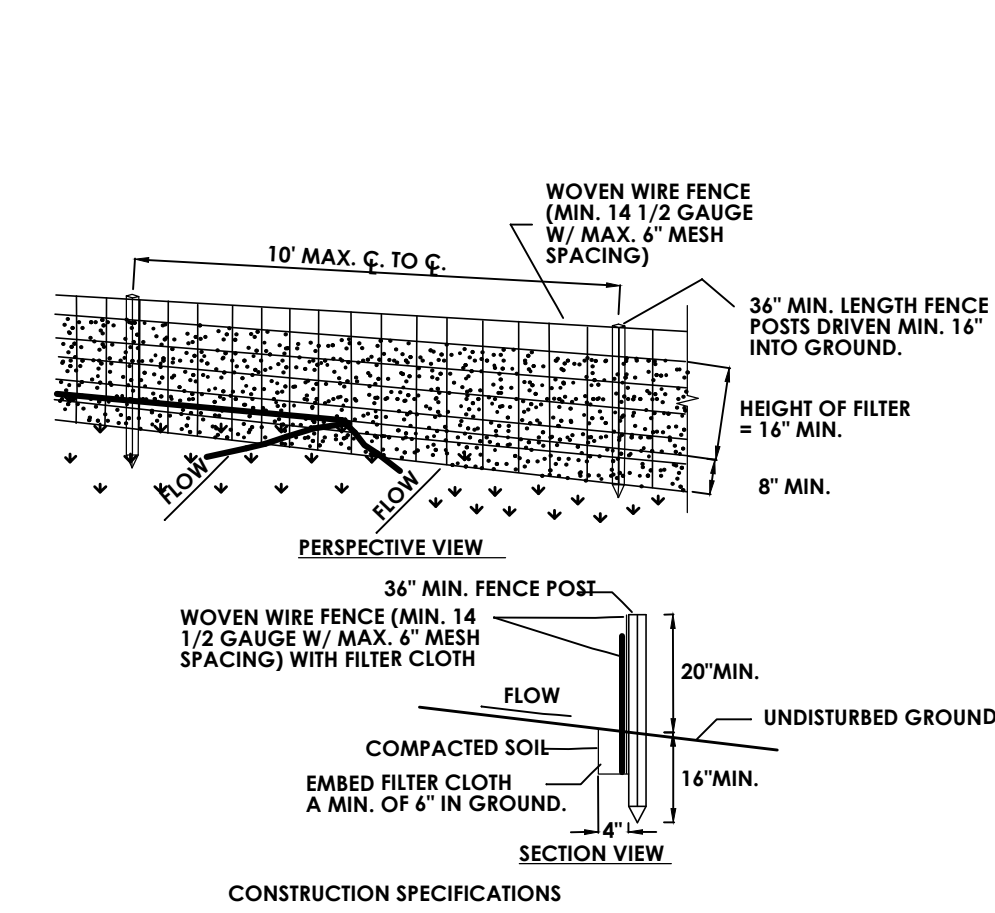
MODIFIED INTERNATIONAL  
SYMBOL OF ACCESS



TYPICAL POST MOUNT  
HANDICAP SIGN INSTALLATION  
(SIGN IN LANDSCAPE AREA OR SIDEWALK)  
N.T.S.

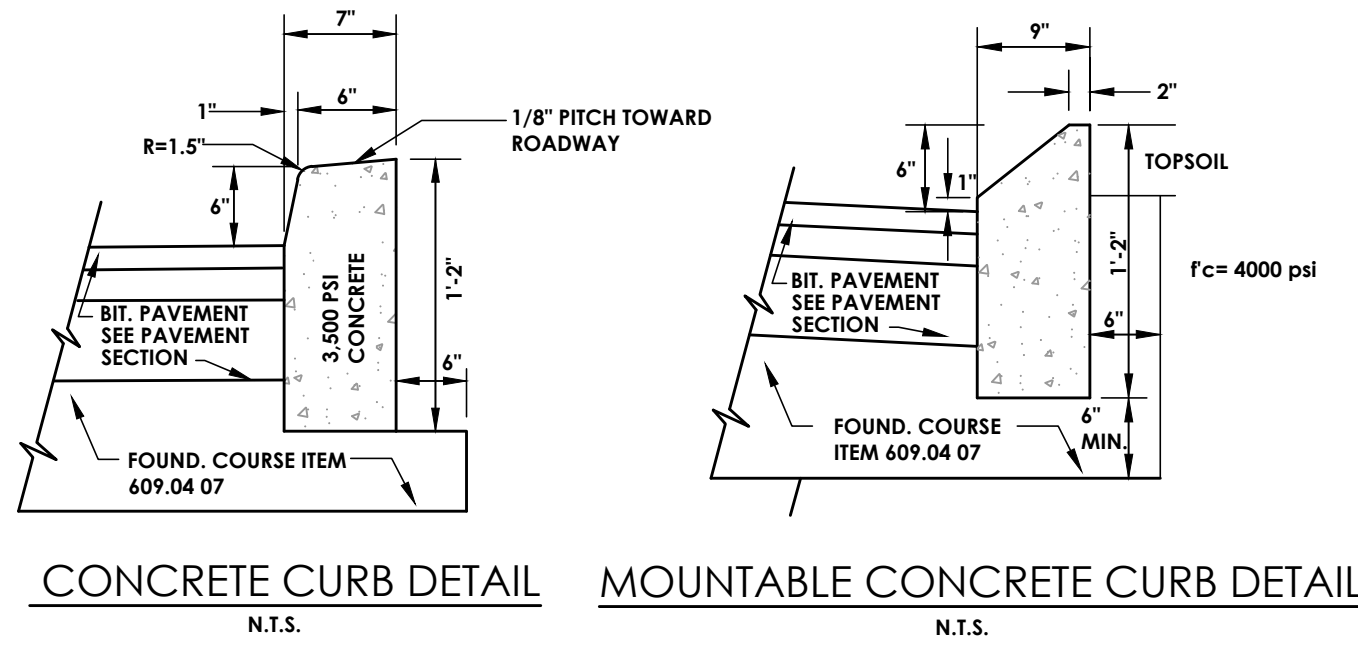


DETECTABLE WARNING SURFACE  
N.T.S.



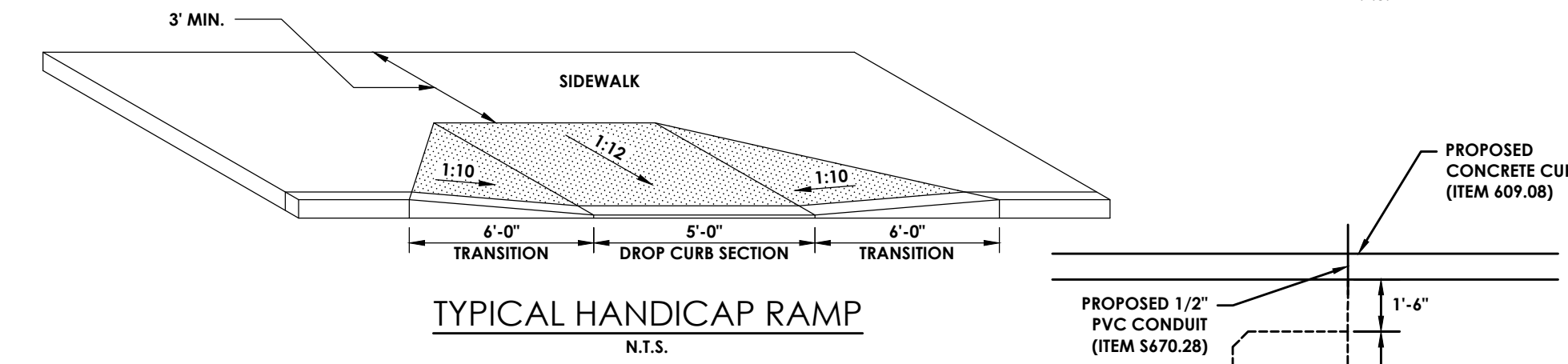
- CONSTRUCTION SPECIFICATIONS
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES. POSTS SHALL BE STEEL EITHER T OR U TYPE OR HARDWOOD.
  2. FILTER CLOTH TO BE TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24\"/>

SILT FENCE DETAIL  
N.T.S.

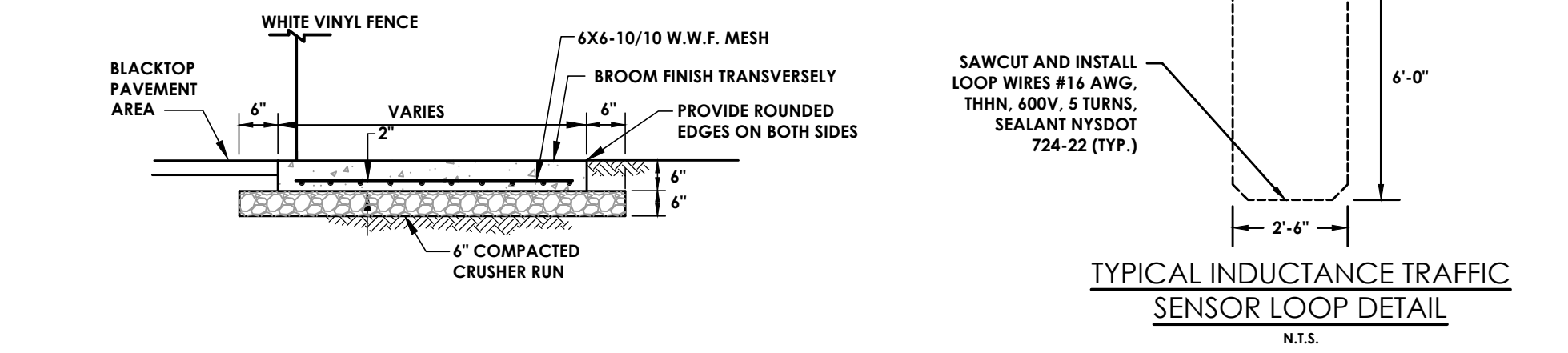


CONCRETE CURB DETAIL  
N.T.S.

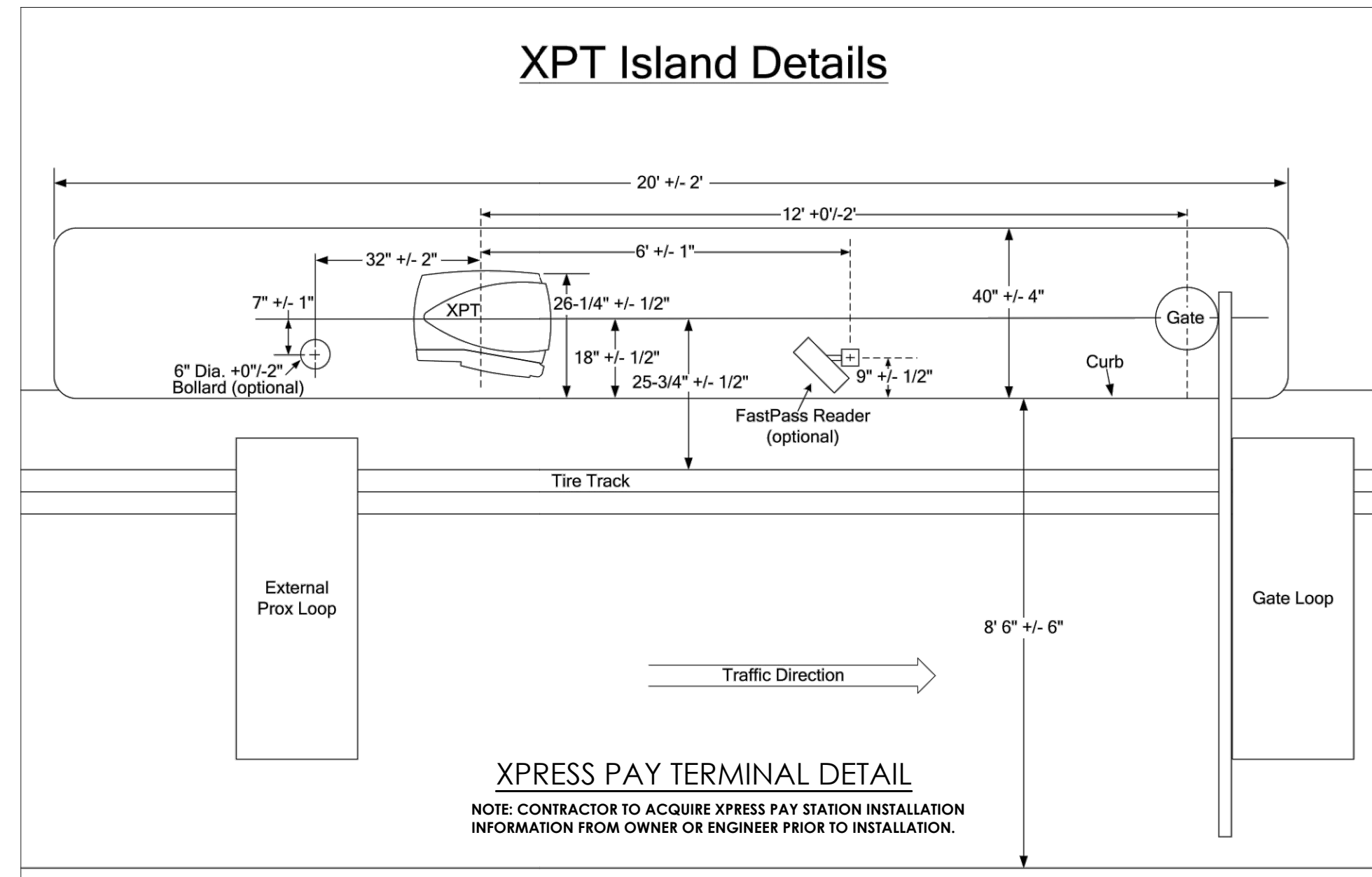
MOUNTABLE CONCRETE CURB DETAIL  
N.T.S.



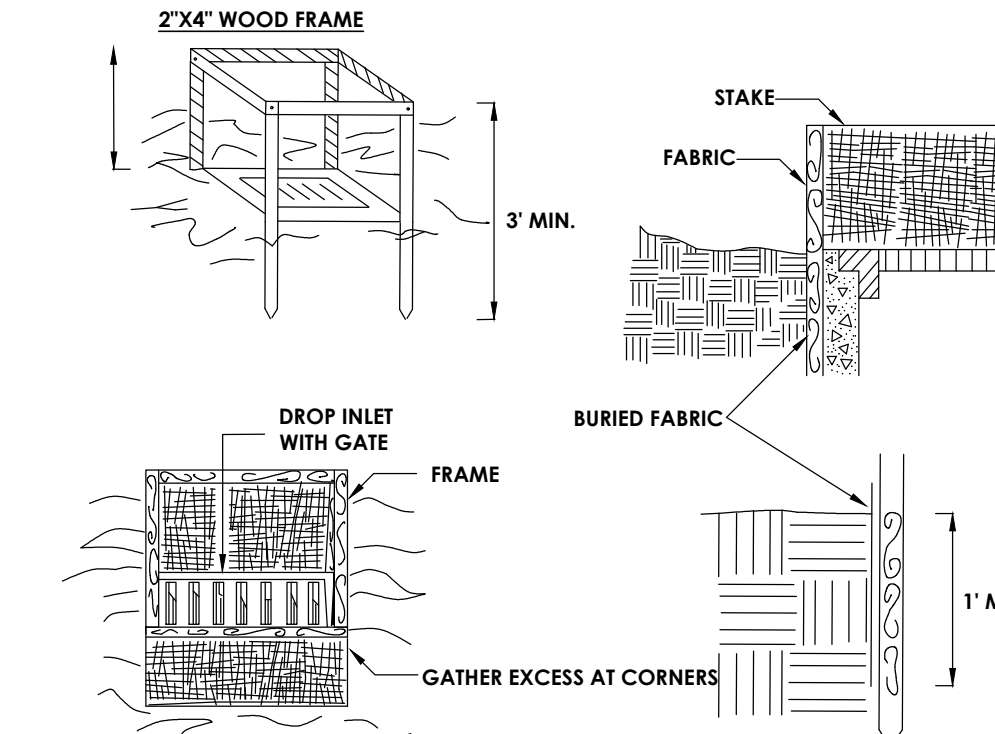
TYPICAL HANDICAP RAMP  
N.T.S.



TYPICAL INDUCTANCE TRAFFIC  
SENSOR LOOP DETAIL  
N.T.S.



XPRESS PAY TERMINAL DETAIL  
NOTE: CONTRACTOR TO ACQUIRE XPRESS PAY STATION INSTALLATION  
INFORMATION FROM OWNER OR ENGINEER PRIOR TO INSTALLATION.

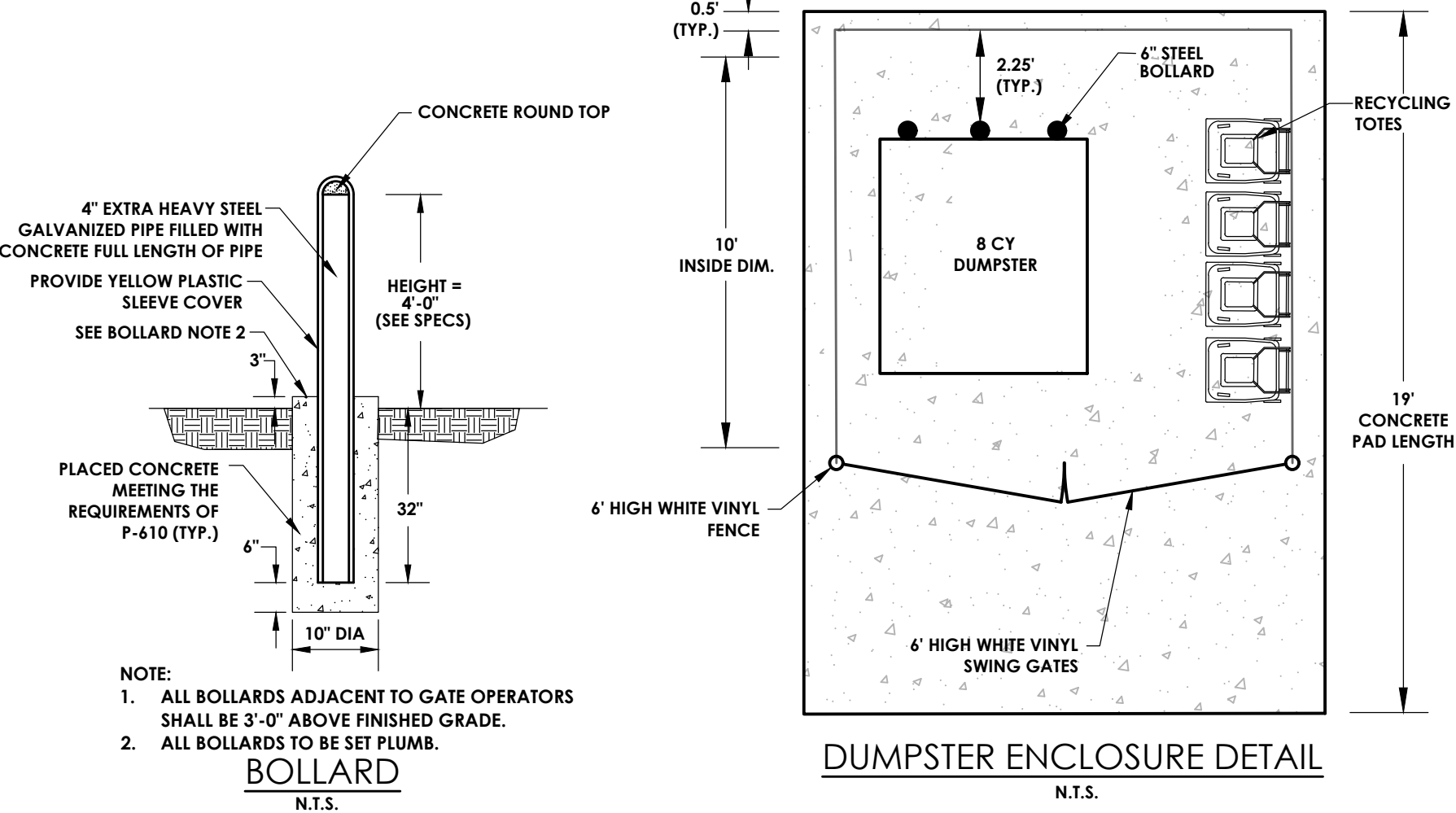


- CONSTRUCTION SPECIFICATIONS
- APPLICATIONS
1. FILTER FABRIC SHALL HAVE AN EOS OF 40-85. BURLAP MAY BE USED FOR SHORT TERM
  2. CUT FABRIC FROM A CONTINUOUS ROLL TO ELIMINATE JOINTS. IF JOINTS ARE NEEDED THEY WILL BE OVERLAPPED TO THE NEXT STAKE.
  3. STAKE MATERIALS WILL BE STANDARD 2\"/>

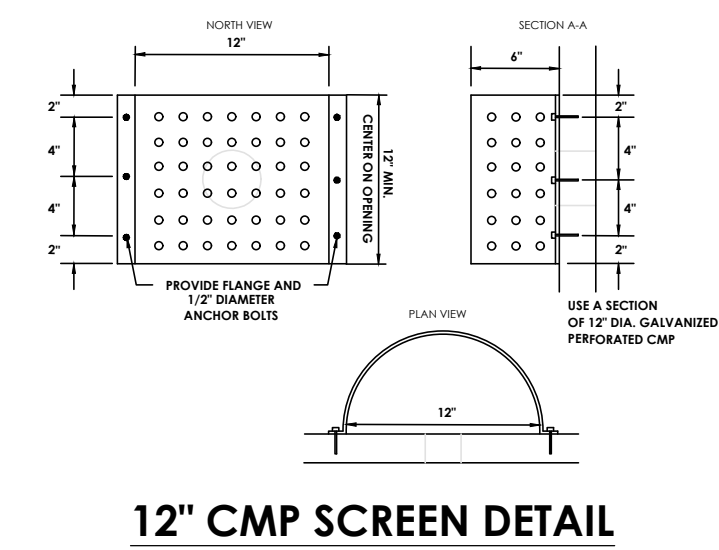
FILTER FABRIC DROP INLET PROTECTION  
N.T.S.

1. CONCRETE SHALL BE 4000 P.S.I. CLASS A\* AIR ENTRAINED CONCRETE.
2. FULL DEPTH EXPANSION JOINTS SHALL BE PROVIDED EVERY 25' DUMMY OR MARKED JOINTS SHALL BE AT 5' FOOT SPACING.

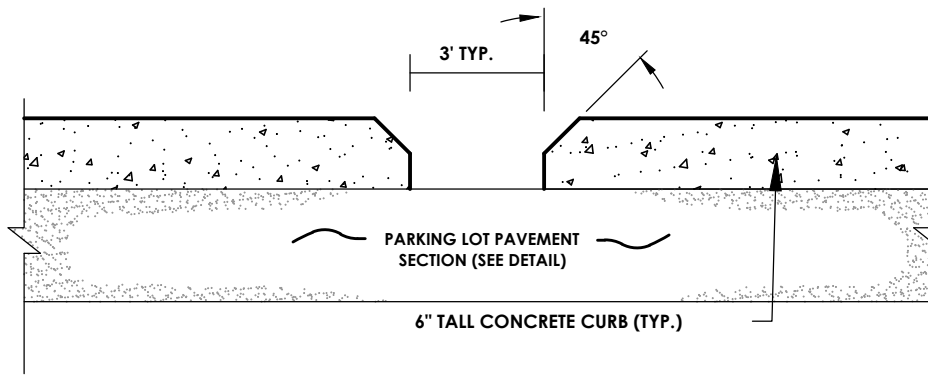
CONCRETE DUMPSTER PAD  
N.T.S.



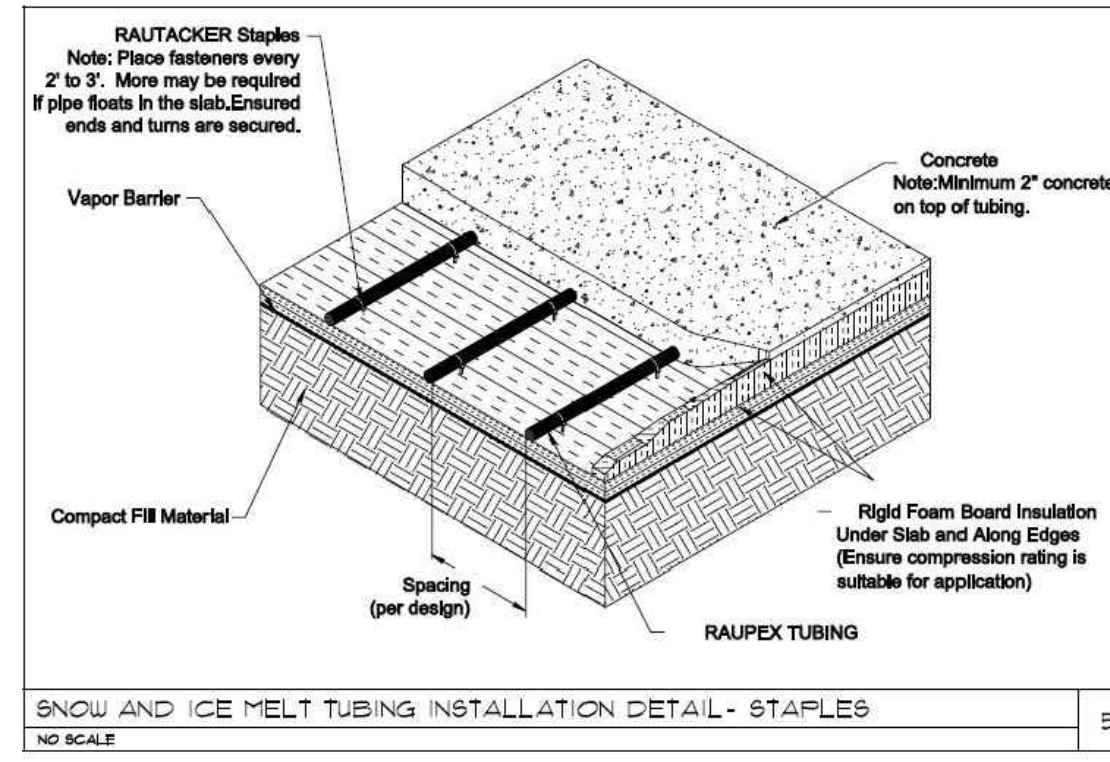
DUMPSTER ENCLOSURE DETAIL  
N.T.S.



12\"/>



DRAINAGE RELIEF IN CURB  
N.T.S.

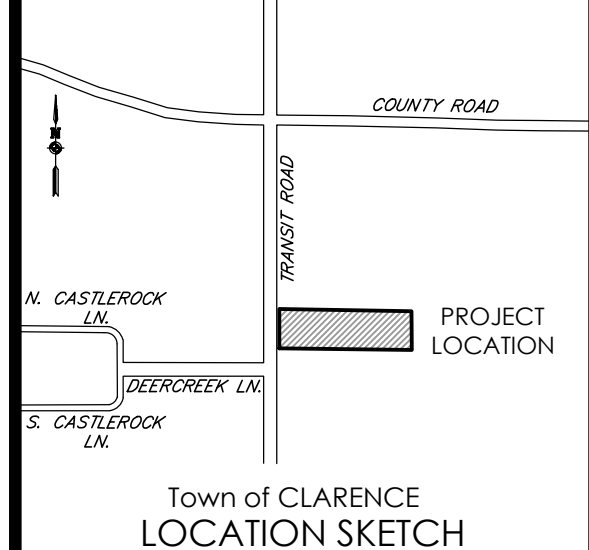


SNOW AND ICE MELT ZONE SCHEDULE - 40% PROPYLENE GLYCOL											
TAG	SERVICE	PRIMARY SPACING (ft.)	PRIMARY AREA (sq.ft.)	HEATING INTENSITY (Btu/hr sq.ft.)	REQUIRED HEAT (Btu/hr)	SUPPLY PLUMB (deg. F)	DELTA T (deg. F)	MANFOLD GPM	HEAD (ft.)	TUBING TYPE	DESIGN EQUIPMENT
ZONE #	ENTRANCE	8	350	150	52,500	120	20	5.25	3.5	3/4" PE-X	REHEAT
	EXIT	8	350	150	52,500	120	20	5.25	3.5	3/4" PE-X	REHEAT

HEATED CONCRETE DETAIL  
N.T.S.

- CONSTRUCTION SPECIFICATIONS
1. STONE SIZE - USE 2\"/>

STABILIZED CONSTRUCTION ENTRANCE  
N.T.S.



Town of CLARENCE  
LOCATION SKETCH

Client:  
**DANIELE FAMILY COMPANIES**  
2851 MONROE AVENUE  
ROCHESTER, NY 14618

**PASSERO ASSOCIATES**  
242 West Main Street Suite 100  
Rochester, New York 14614  
(585) 325-1000  
Fax: (585) 325-1691

Principal-in-Charge: **Jess Sudol, PE**  
Project Manager: **David Cox, PE**  
Designed by: **James Ritzenhaler**



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5	12/05/22	ABC	PER TOWN ENGINEER COMMENTS

DETAILS

**RWD CLARENCE**

County: ERIE Town/City: CLARENCE State: NEW YORK

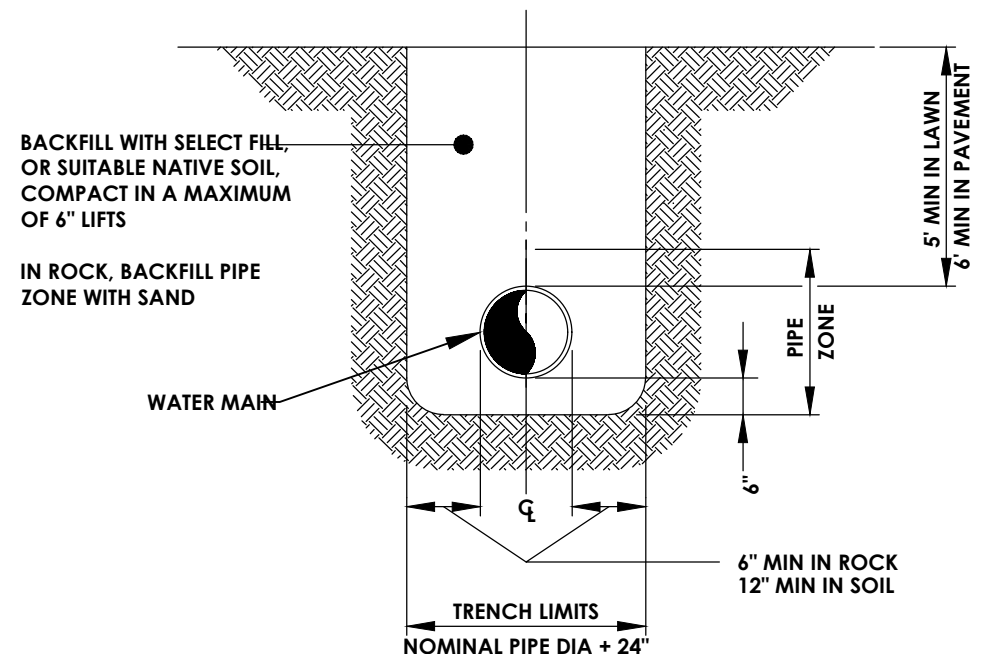
Project No.: **20213204.0001**

Drawing No.: **C 202** Sheet No.: **10**

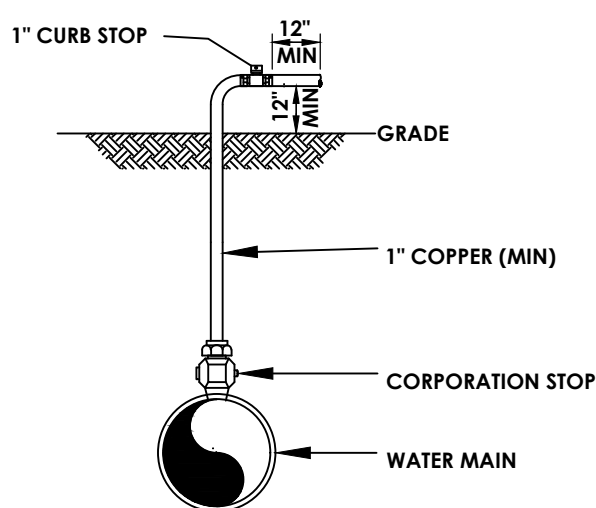
Scale: **N.T.S.**

Date: **SEPTEMBER 2021**



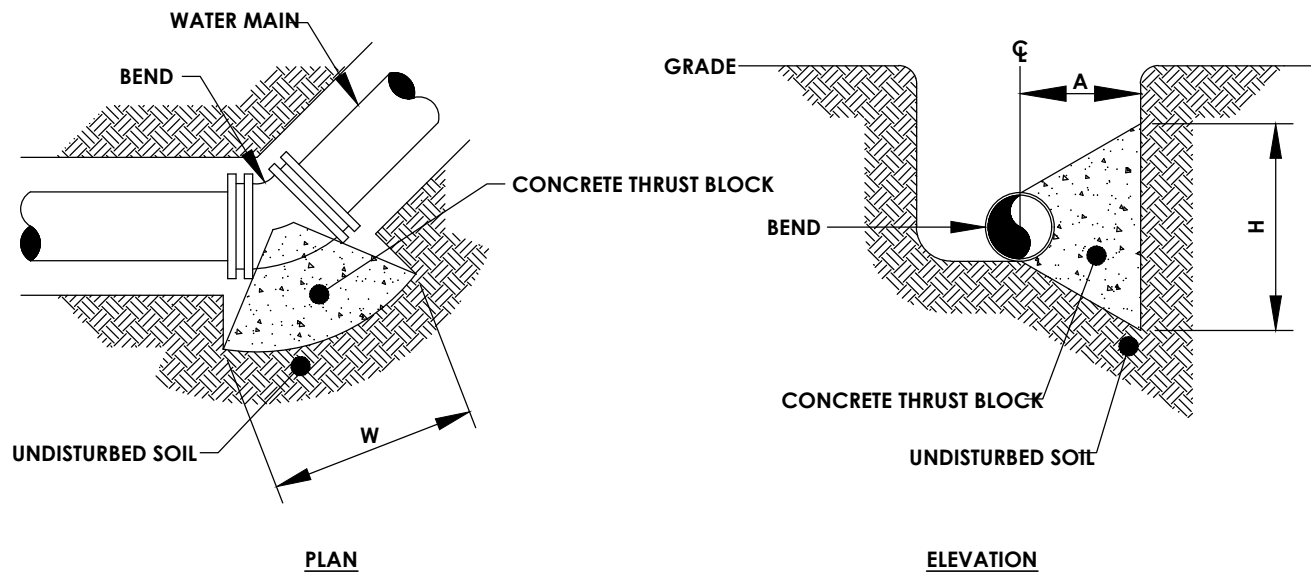
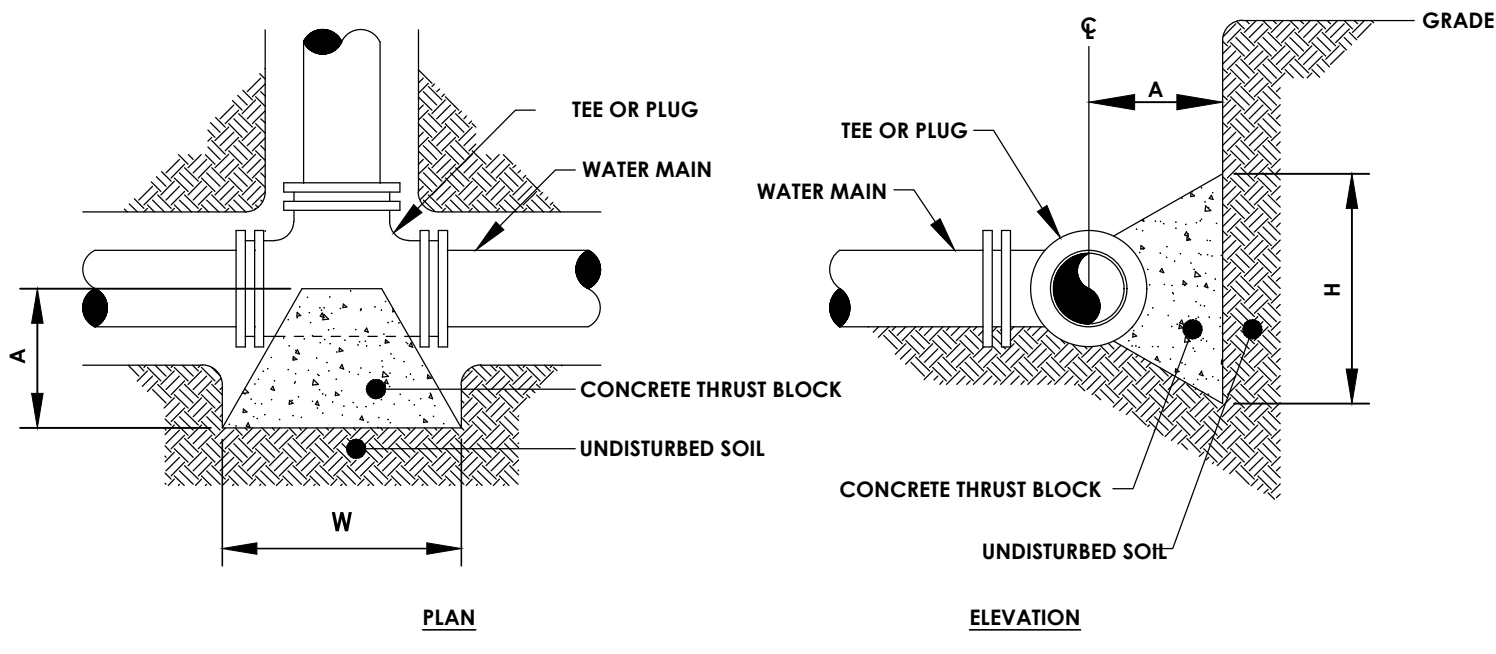


**WATERMAIN TRENCH**  
N.T.S.



**NOTE:**  
IMMEDIATELY PRIOR TO THE AUTHORITY PLACING WATER MAIN IN SERVICE CONTRACTOR SHALL REMOVE ALL CORPORATIONS ASSOCIATED WITH TEMPORARY FACILITIES (I.E. SAMPLING TAPS, ETC.) AND REPLACE WITH THREADED BRASS PLUGS. PLACEMENT OF THREADED BRASS PLUG MUST BE WITNESSED BY A ECWA REPRESENTATIVE.

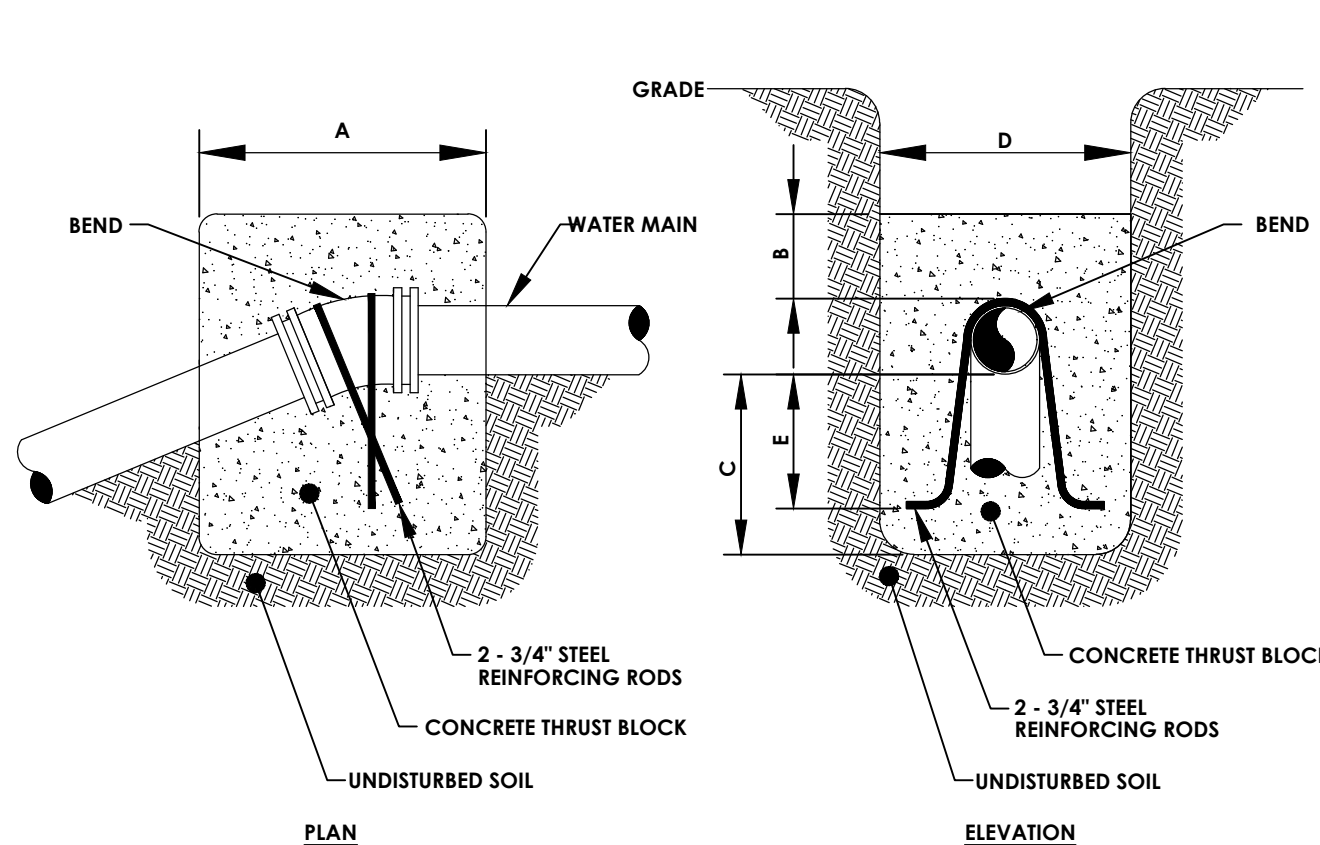
**DISINFECTION/BLOW-OFF/SAMPLING TAP**



MINIMUM HORIZONTAL THRUST BLOCK DIMENSIONS, IN FEET, TO BE POURED AGAINST UNDISTURBED SOIL			
FITTING	H	W	A
4" x 11-1/4" BEND	0.5	1.0	0.7
4" x 22-1/2" BEND	0.7	1.5	0.7
4" x 45" BEND	1.0	2.0	1.0
4" x 90" BEND	1.5	3.0	1.2
4" TEE OR PLUG	1.2	2.5	1.0

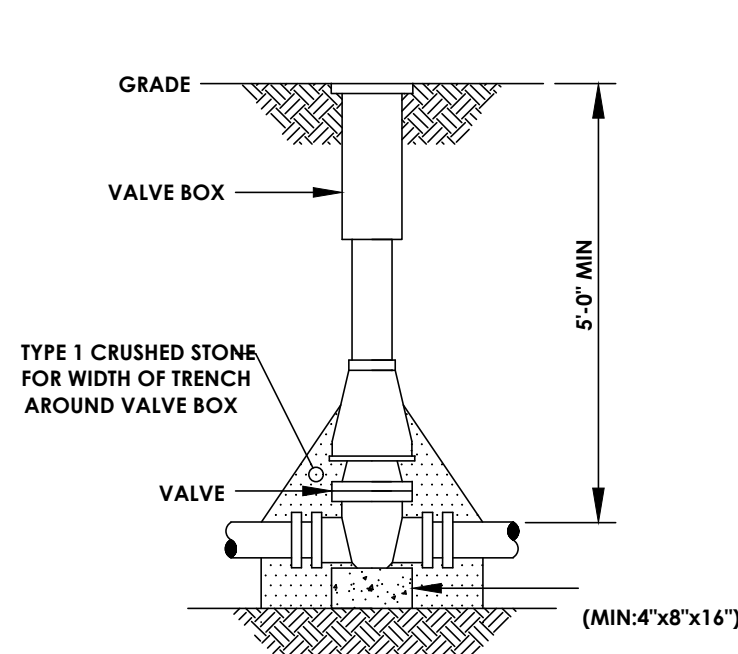
**NOTE:** WIDTH (W) OF BLOCK SHALL NOT EXCEED TWICE THE HEIGHT (H).

**HORIZONTAL THRUST BLOCK FOR BENDS**  
N.T.S.



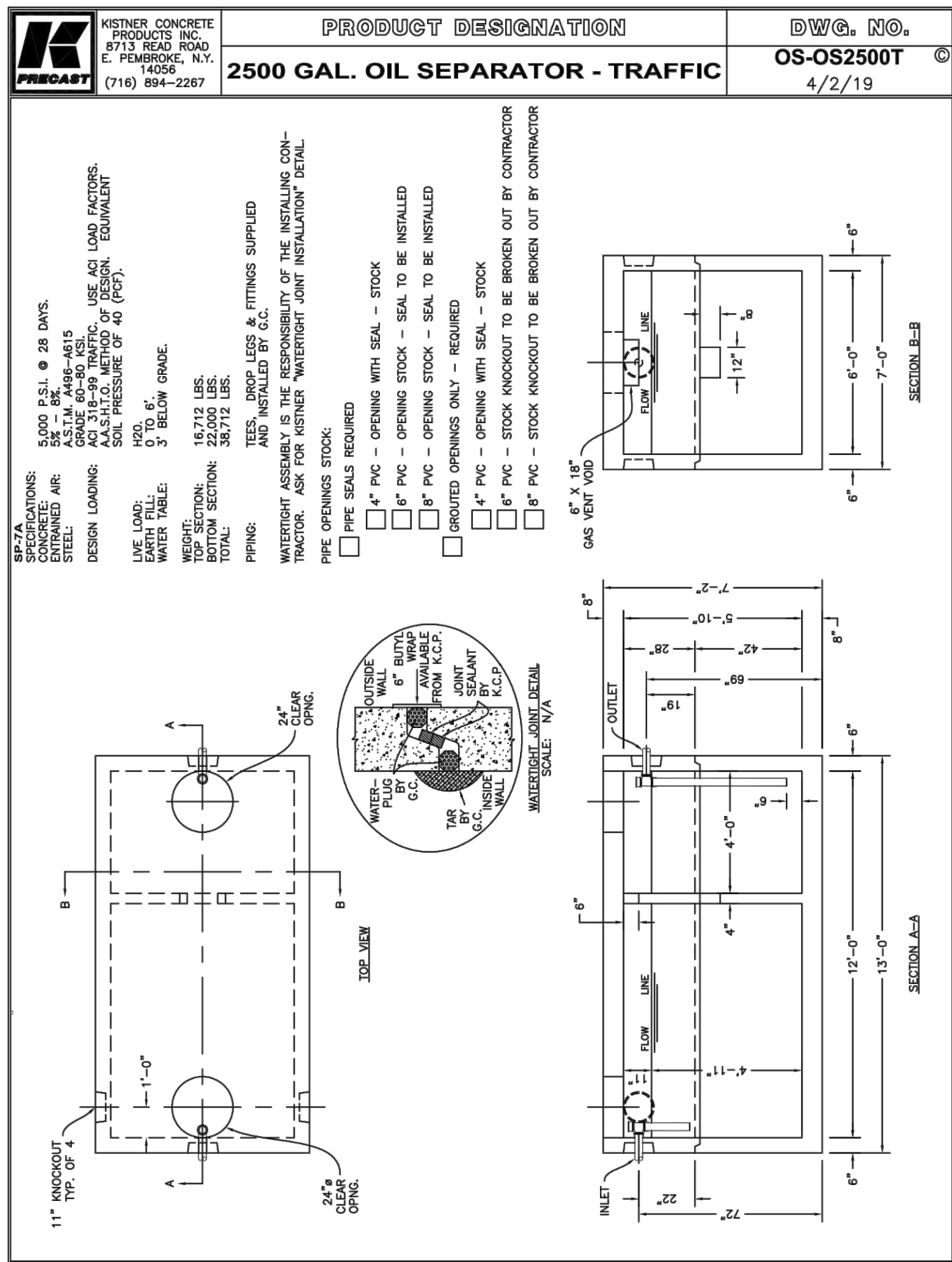
BEND *	MINIMUM VOLUME OF CONCRETE DIMENSIONS	MINIMUM ALLOWABLE DIMENSIONS FOR VERTICAL THRUST BLOCKS (IN FEET)				
		A	B	C	D	E
4" x 11-1/4"	0.35 C.Y.	0.5	1.2	0.5	1.0	0.5
4" x 22-1/2"	0.70 C.Y.	0.5	1.2	0.7	1.0	0.5
4" x 45"	1.40 C.Y.	0.7	1.2	0.7	1.0	0.5

**VERTICAL THRUST BLOCK**  
N.T.S.

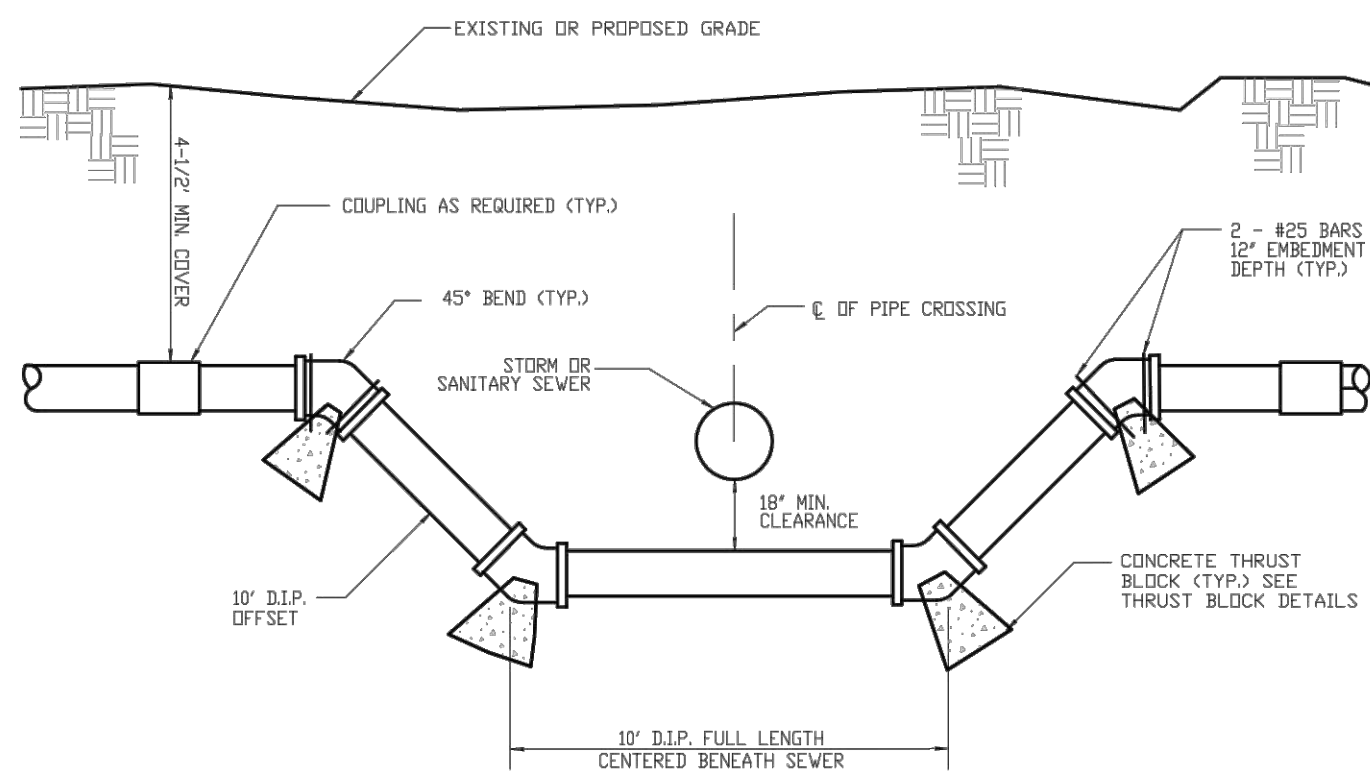


- NOTE:**
1. VALVE BOX SHALL BE CENTERED ON VALVE AND SET ON COMPACTED BACKFILL.
  2. VALVE SHALL NOT SUPPORT VALVE BOX.
  3. ALL BODY AND BONNET BOLTS SHALL BE STAINLESS STEEL.
  4. ALL VALVES SHALL BE OPEN LEFT EXCEPT VALVES 12" AND SMALLER

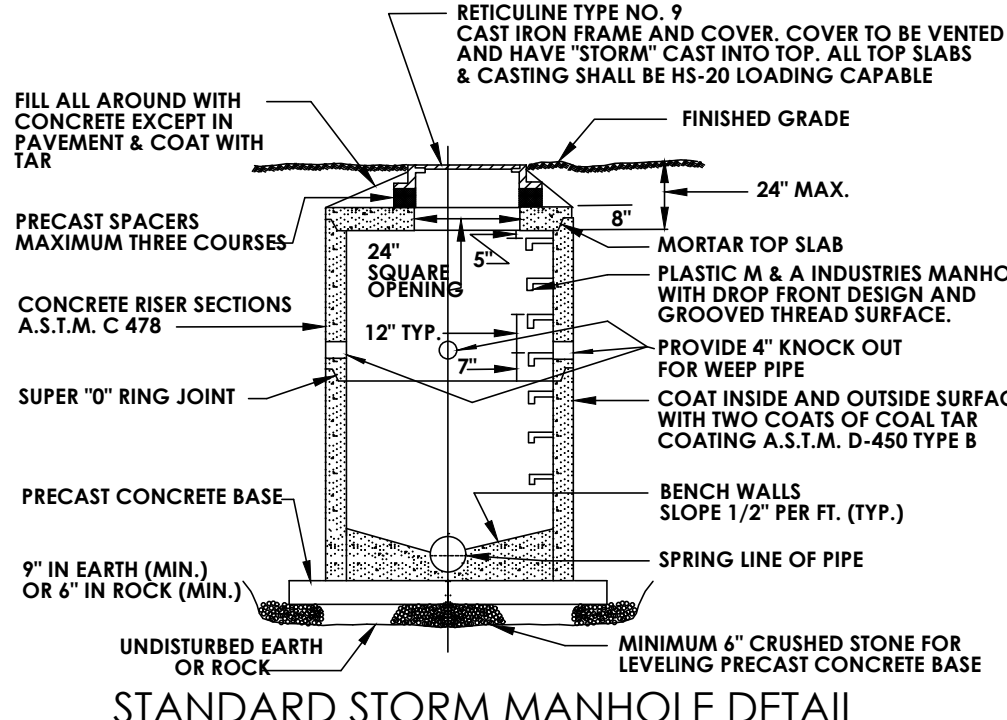
**WATER VALVE**  
N.T.S.



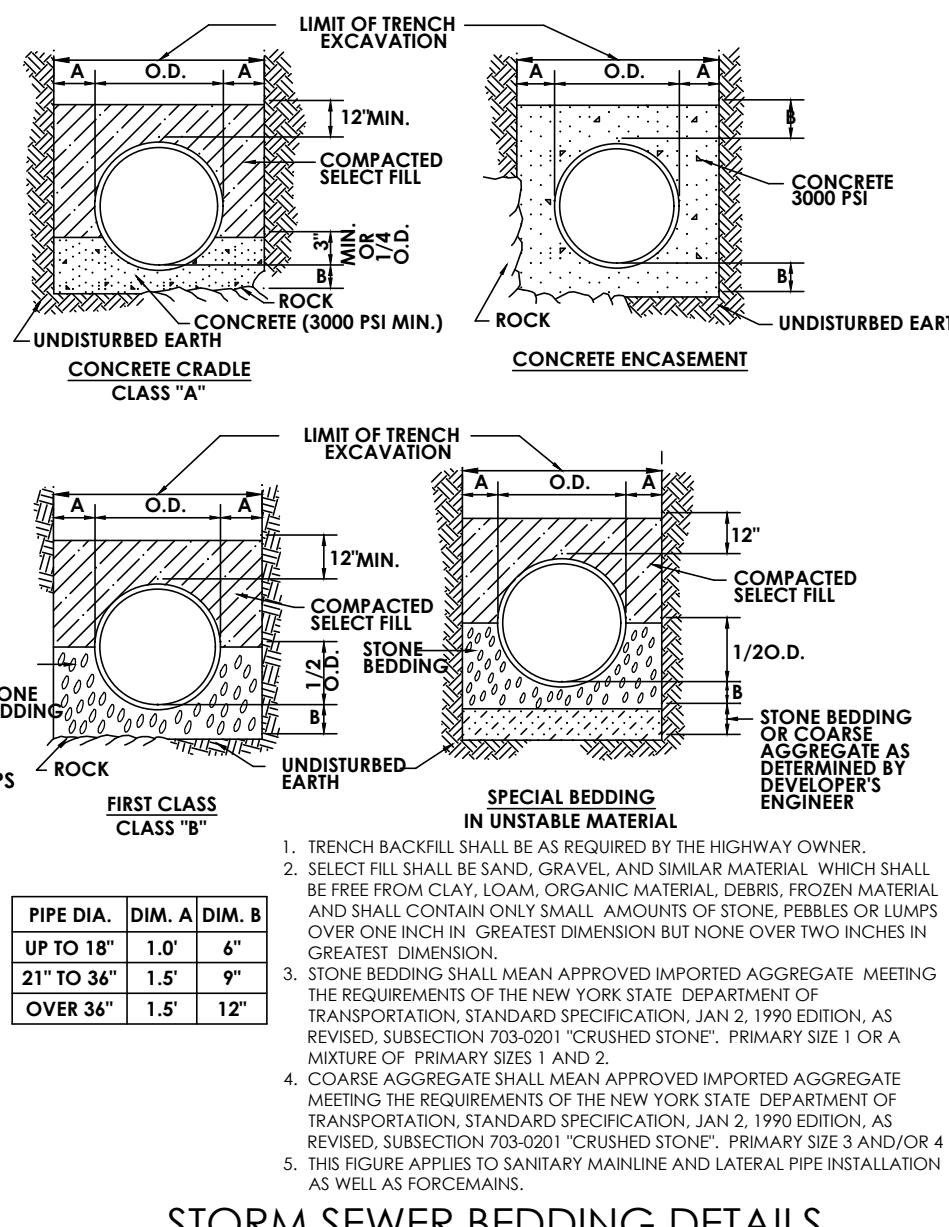
**2,500 GALLON OIL WATER SEPARATOR**



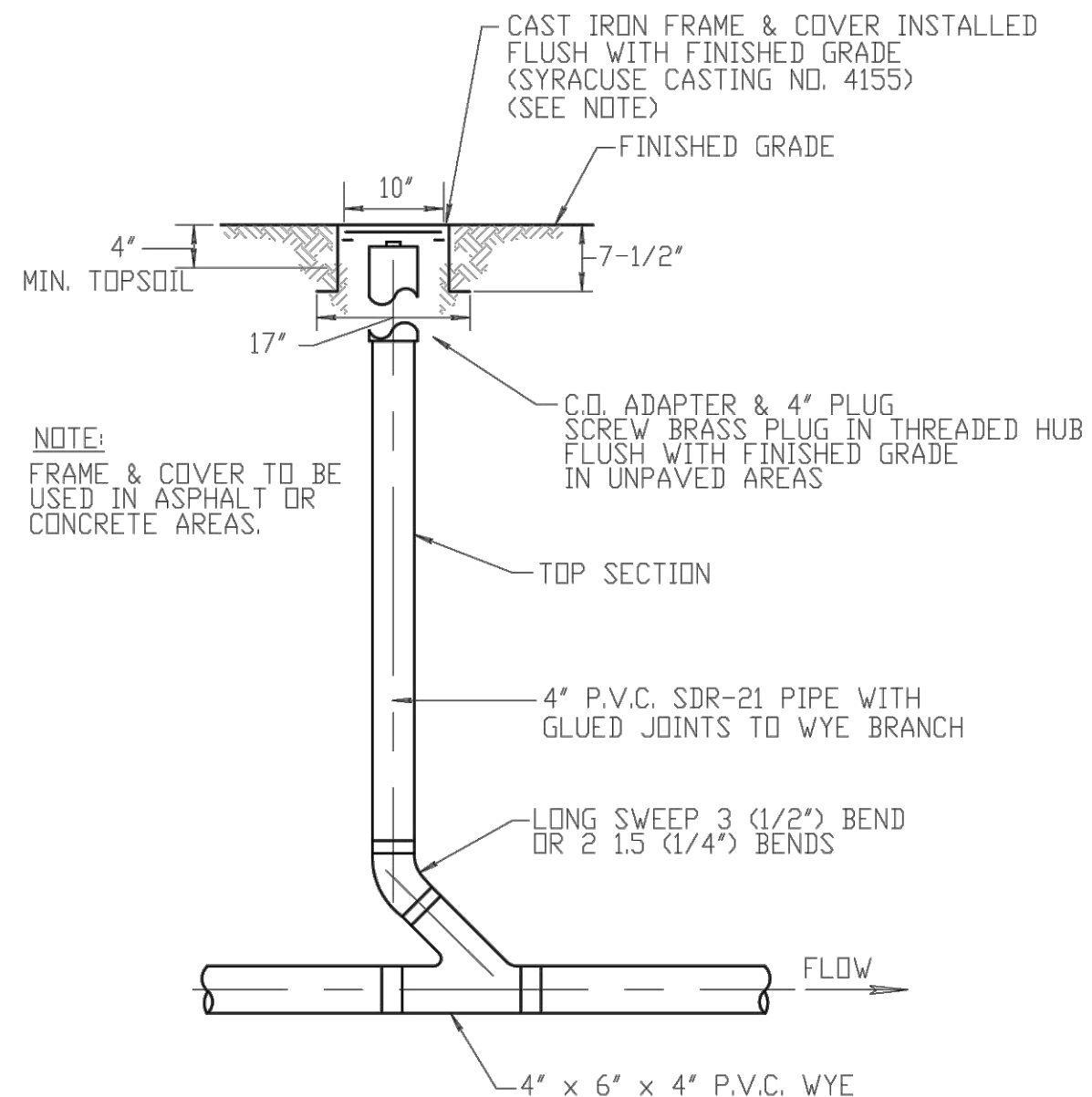
**WATERMAIN - SEWER CROSSING**  
ITEM 660.99  
NOT TO SCALE



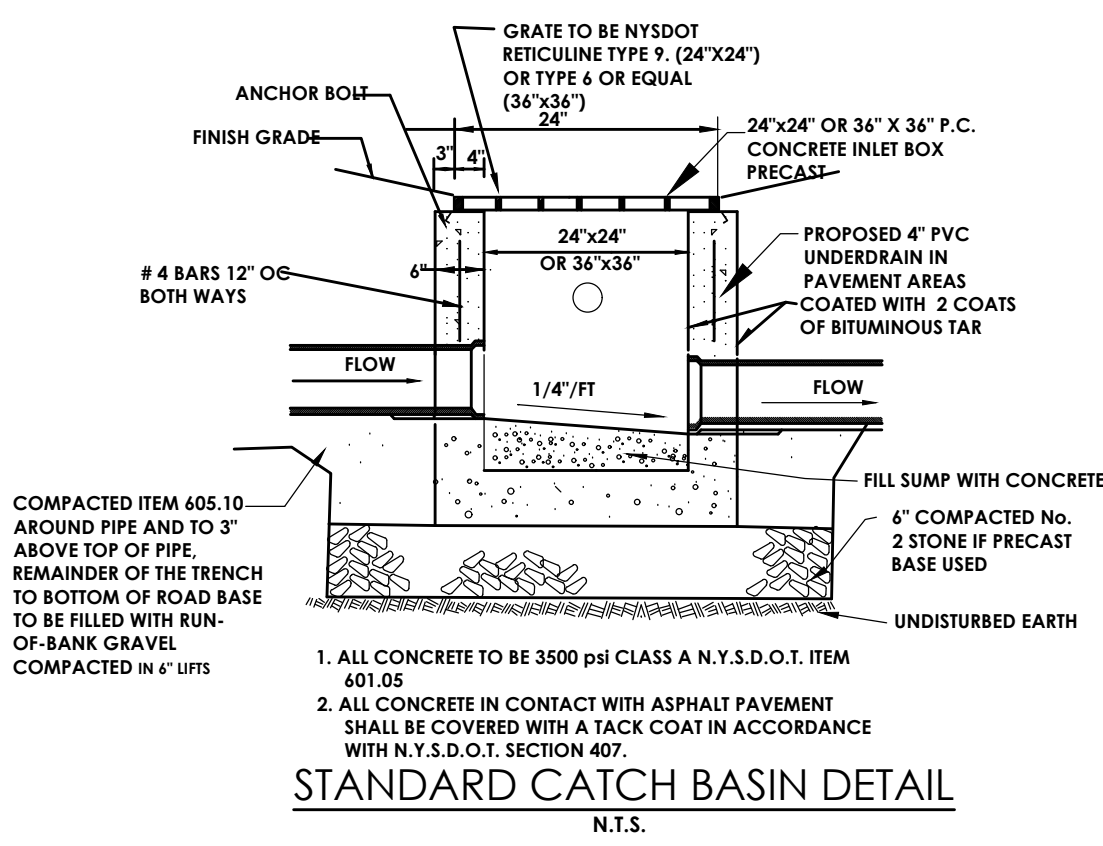
**STANDARD STORM MANHOLE DETAIL**  
N.T.S.



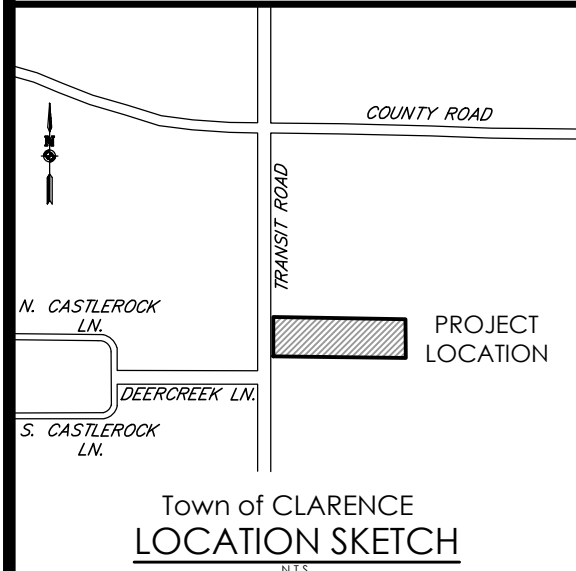
**STORM SEWER BEDDING DETAILS**  
N.T.S.



**CLEANOUT IN-LINE**  
NOT TO SCALE



**STANDARD CATCH BASIN DETAIL**  
N.T.S.



Town of CLARENCE  
**LOCATION SKETCH**

Client:  
**DANIELE FAMILY COMPANIES**  
2851 MONROE AVENUE  
ROCHESTER, NY 14618

**PASSERO ASSOCIATES**  
242 West Main Street Suite 100  
Rochester, New York 14614  
(585) 325-1000  
Fax: (585) 325-1691

Principal-in-Charge: **Jess Sudol, PE**  
Project Manager: **David Cox, PE**  
Designed by: **James Ritzenhaler**



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**DETAILS**

**RWD CLARENCE**

Town/City: CLARENCE  
County: ERIE State: NEW YORK

Project No.: **20213204.0001**

Drawing No.: **C 203** Sheet No.: **11**

Scale: **N.T.S.**

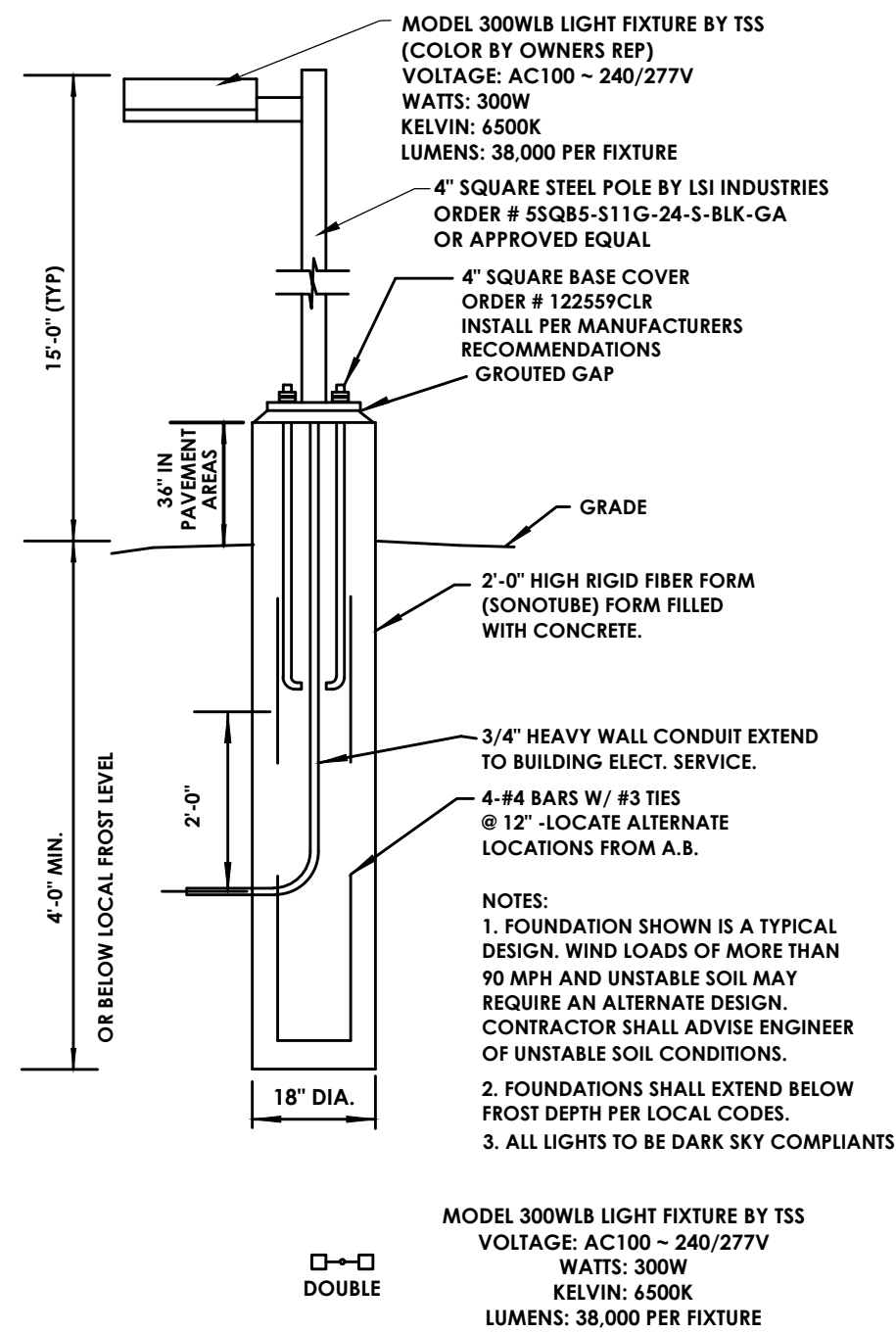
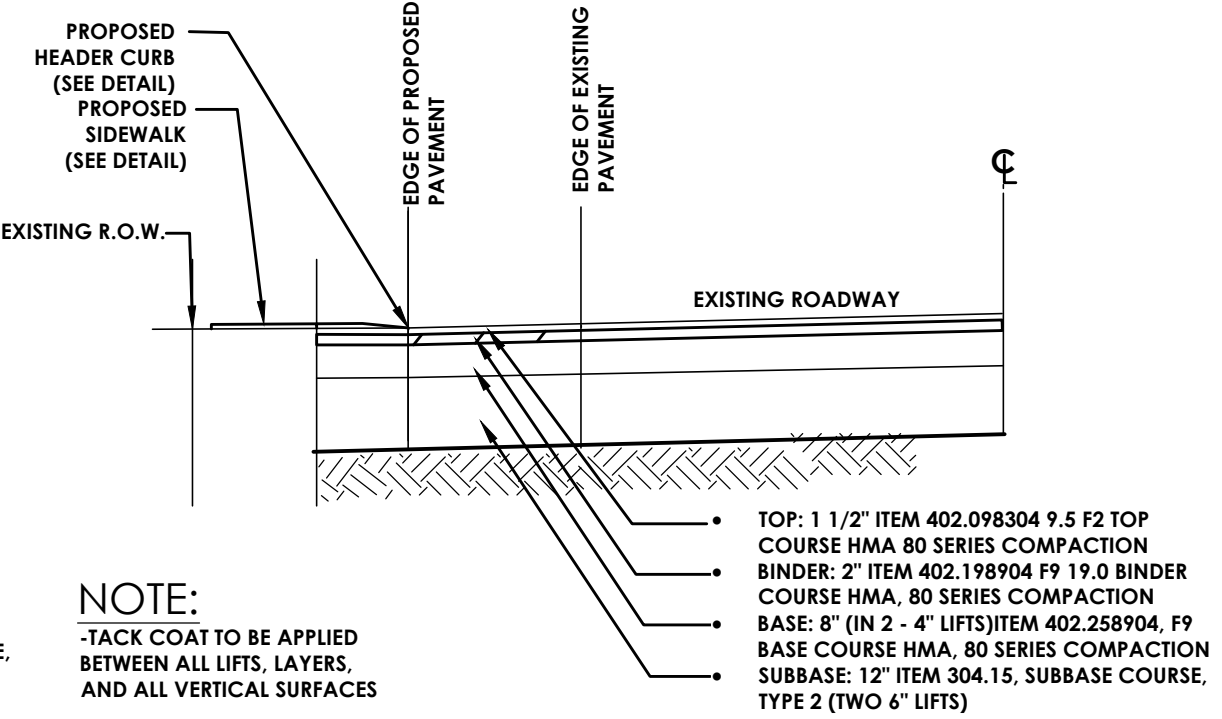
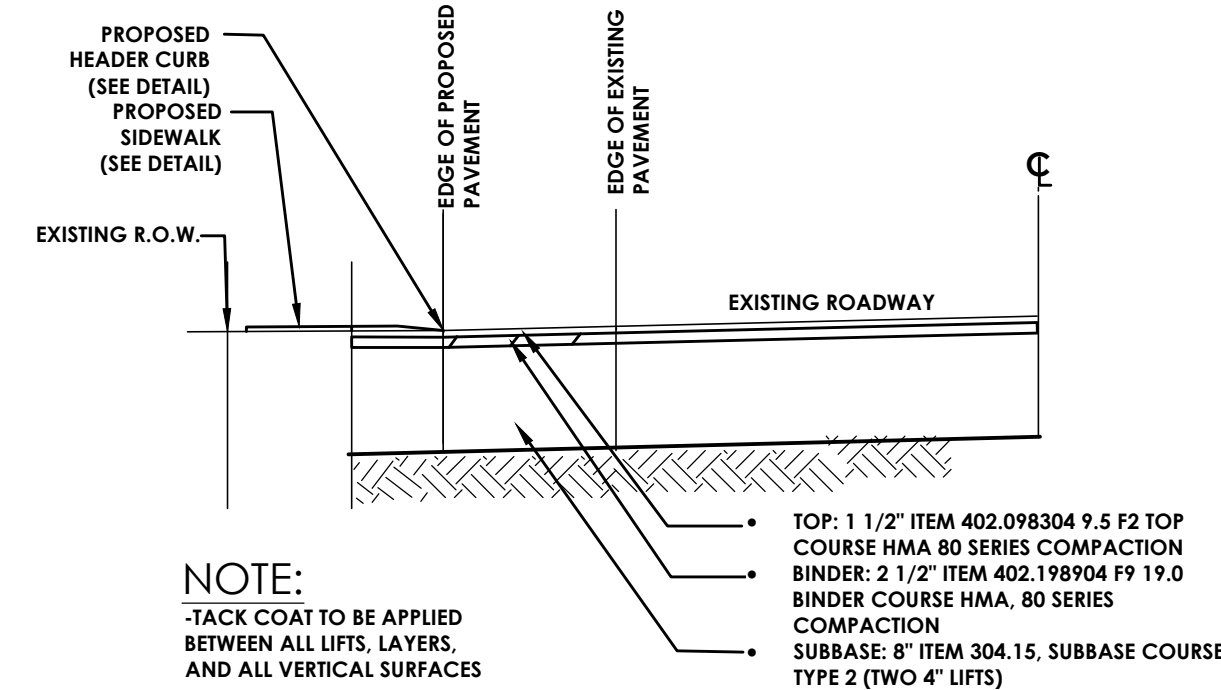
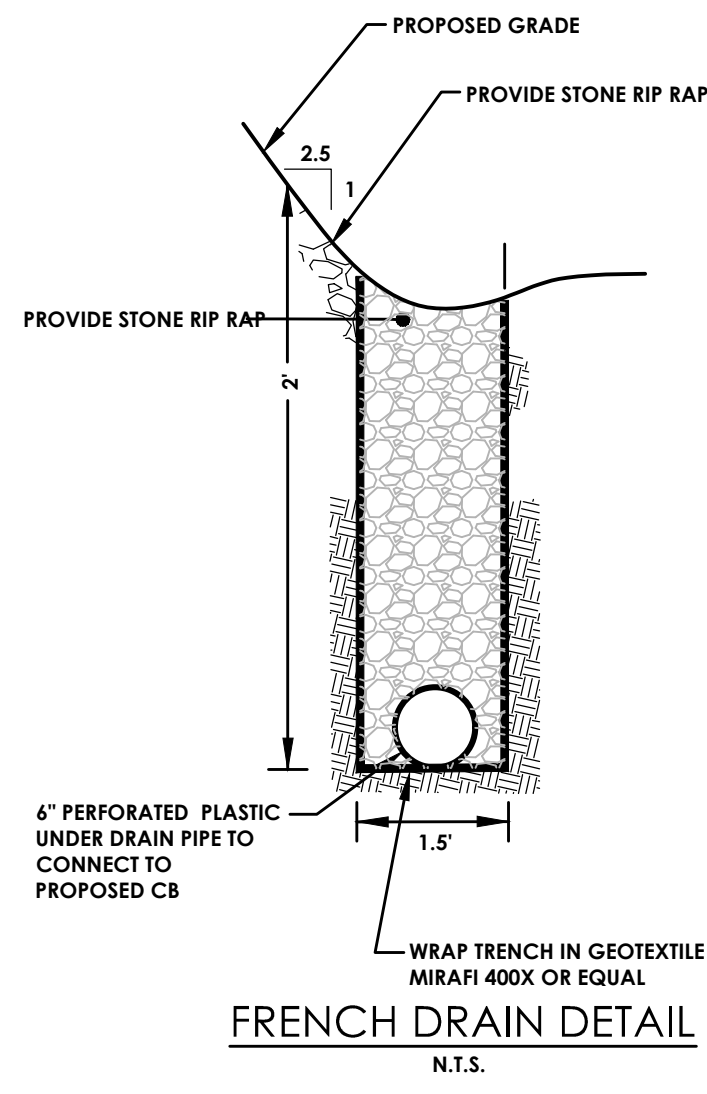
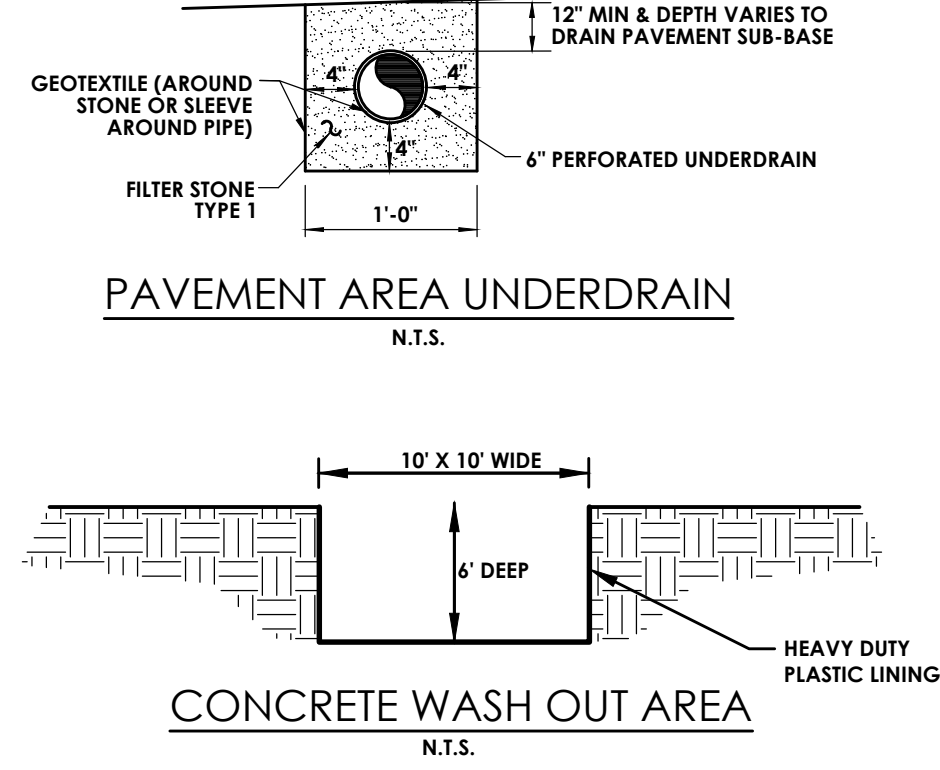
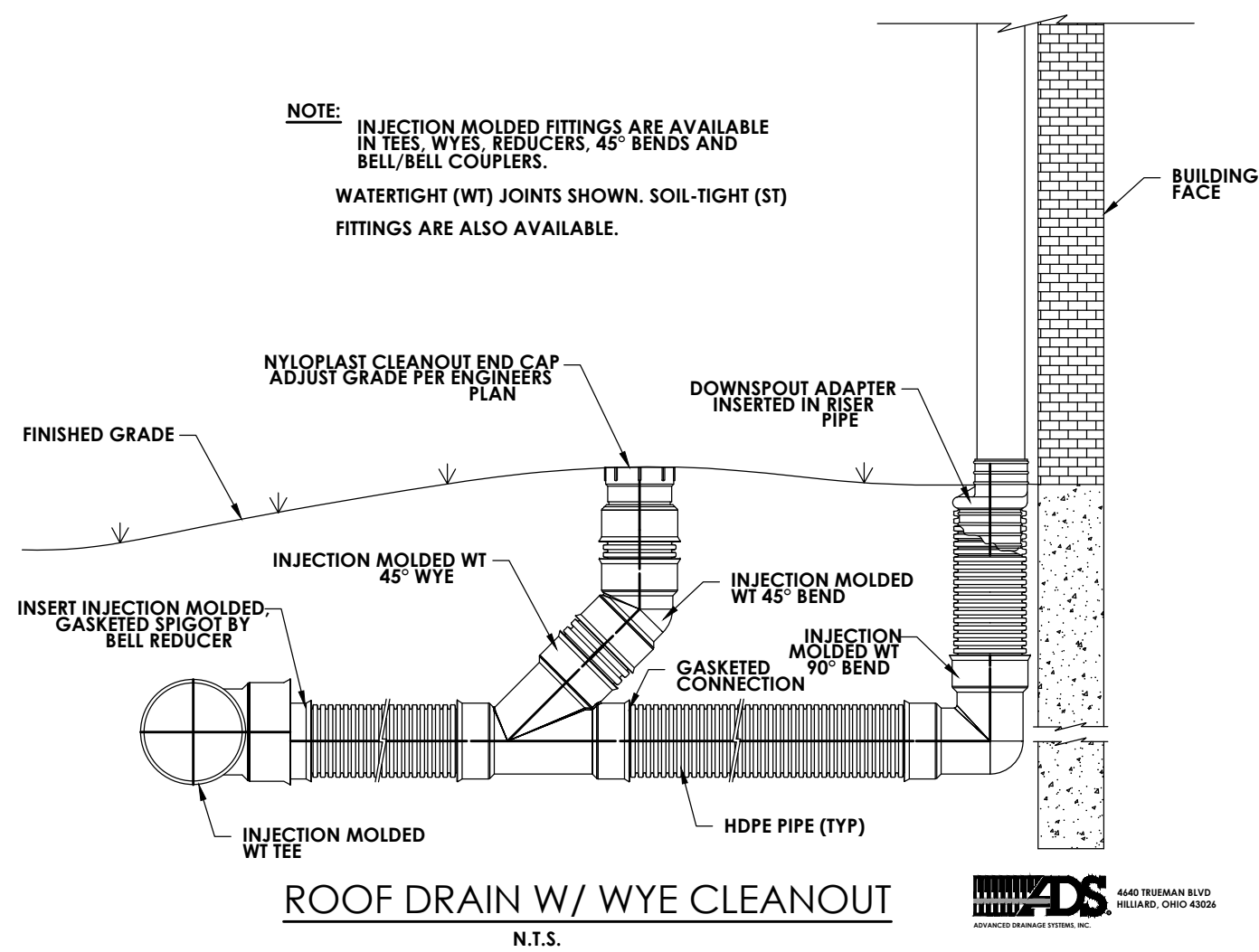
Date: **SEPTEMBER 2021**

NOT FOR CONSTRUCTION









### 300 WATT LED HIGH OUTPUT PARKING LOT LIGHT REPLACES 1000 WATT METAL HALIDE



#### Features

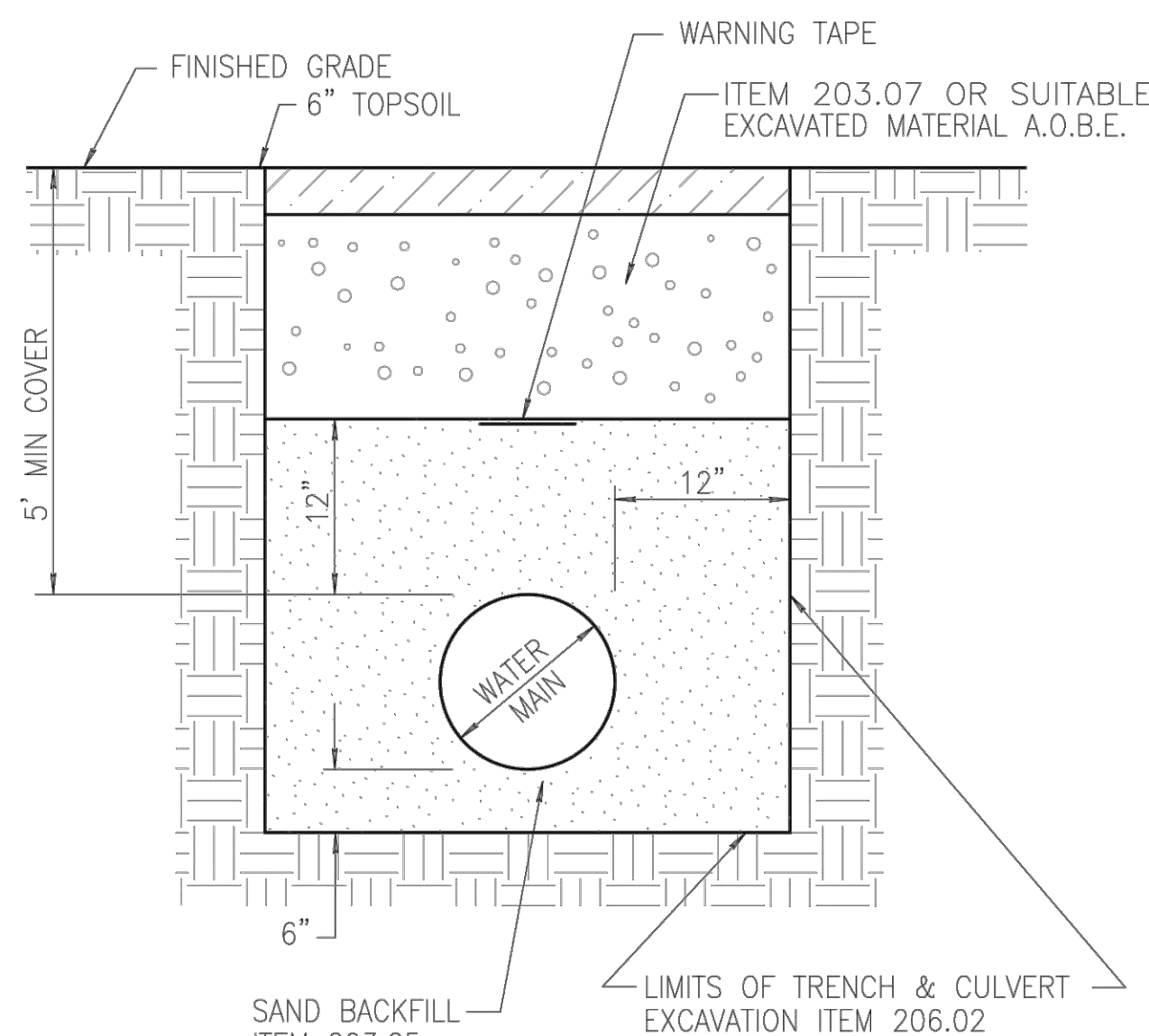
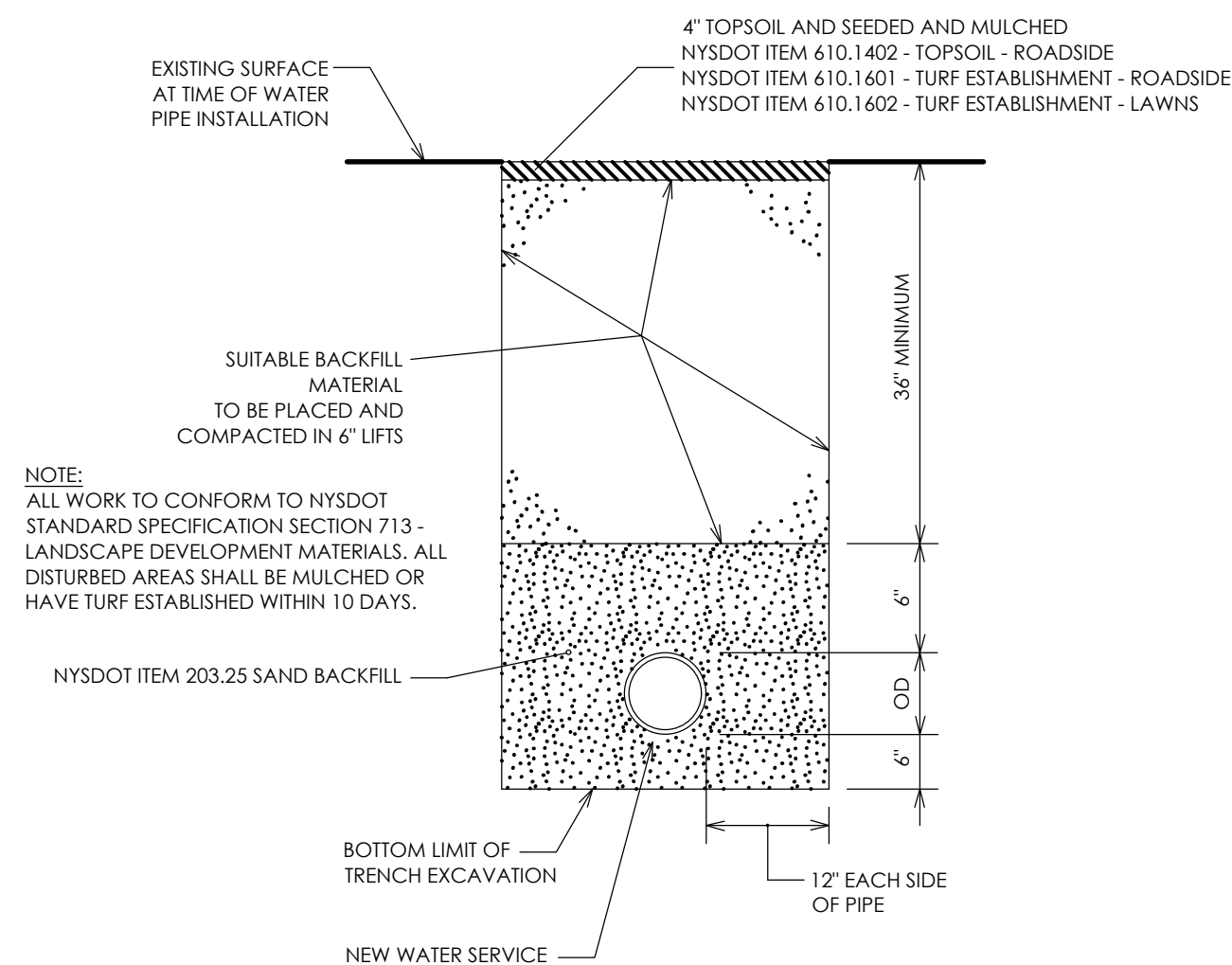
- IP67 waterproof, wet location.
- Gives a crisp and bright lighting area that is great for surveillance camera recordings.
- Photocell control is optional (see below)
- UL and DLC listed.
- 2 Year Limited Warranty.

#### Specifications

Dimensions	25.3" L X 15.9" W X 3.4" H
Voltage	110VAC ~ 240VAC
Watts	300W
Construction	Aluminum housing with polycarbonate lens
Mounting	Pole mount or optional wall mount adapter (see Step 1 next page)
Kelvin	6500 K
Lumens	38,000 per fixture

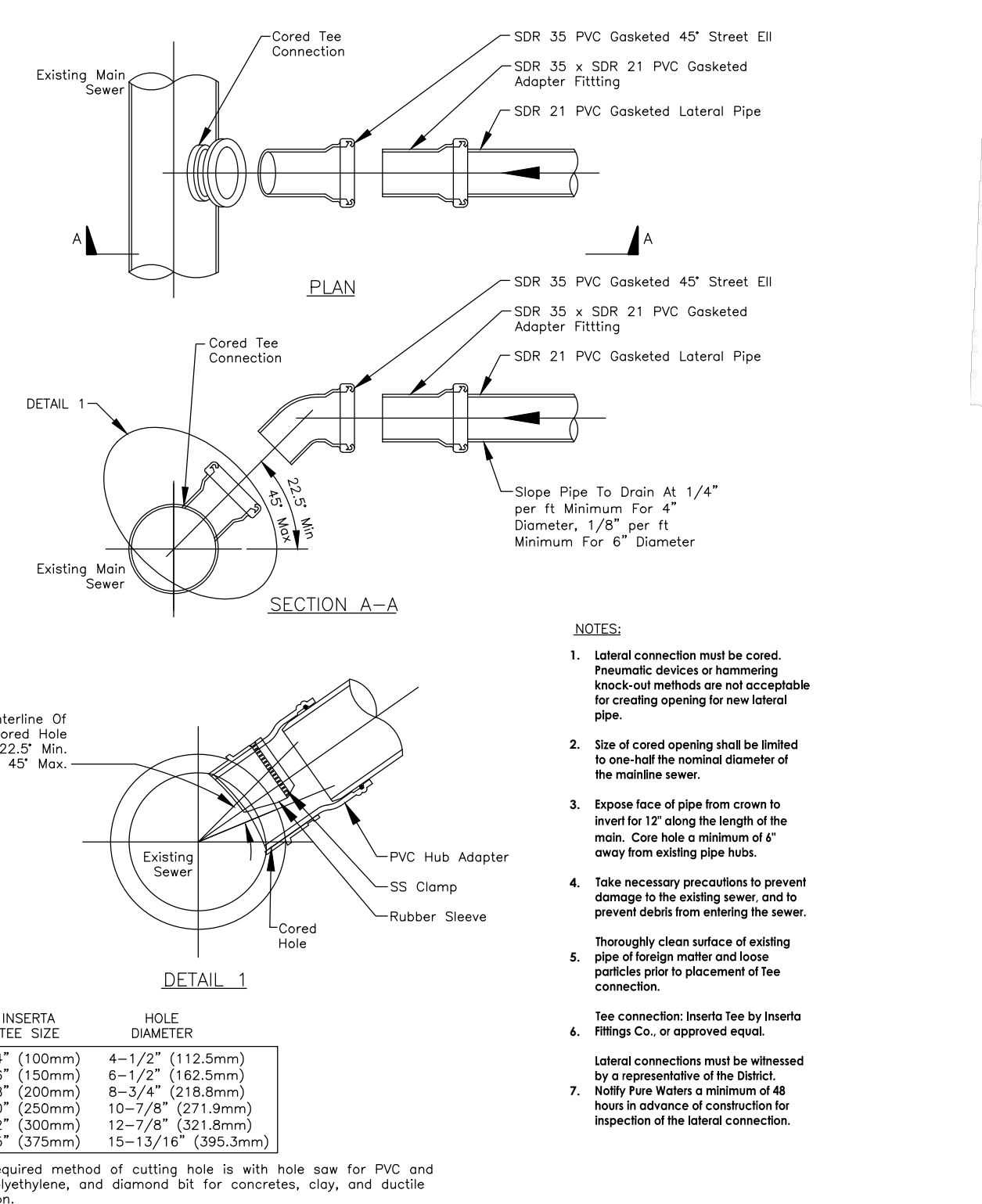
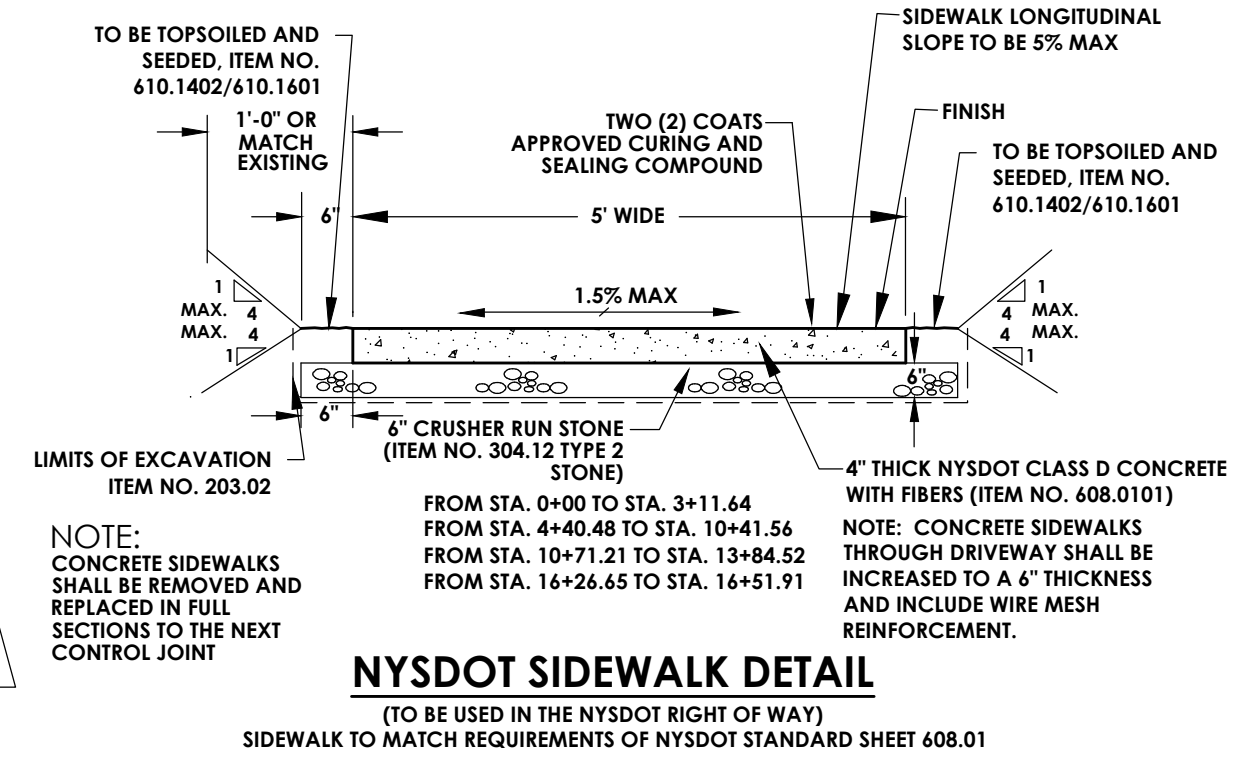
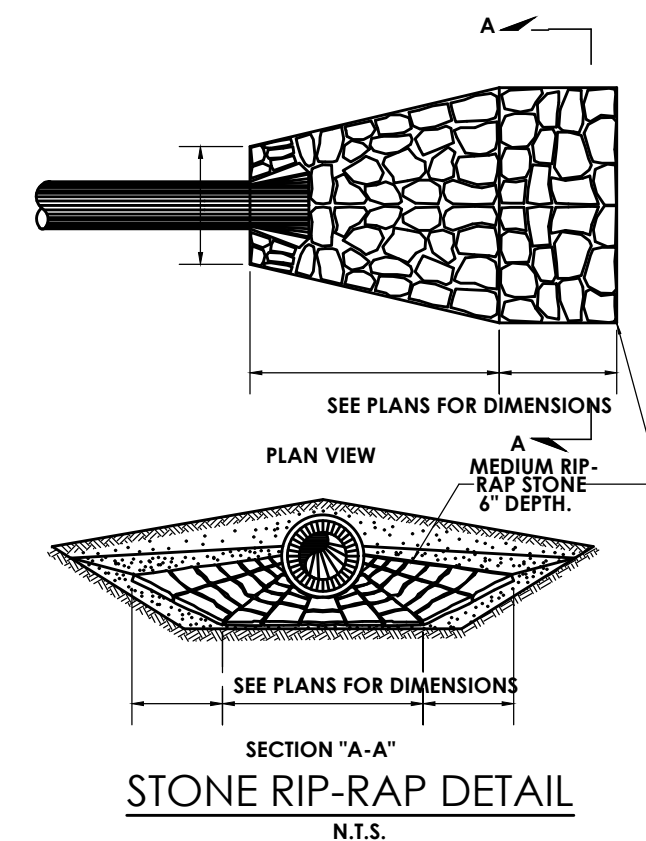
Item	Description	Lumens	Watts	Kelvin	Price
300WLB	300 WATT LED parking lot light with 2 3/8" diameter opening for round pole mount	38,000	300W	6500K	\$446.25
WMAM	Aluminum Flat Wall mount adapter				\$31.50
LEDPS	Photo Eye				\$26.24

This product may be subjected to a potential recall fee exchange - call for current status.



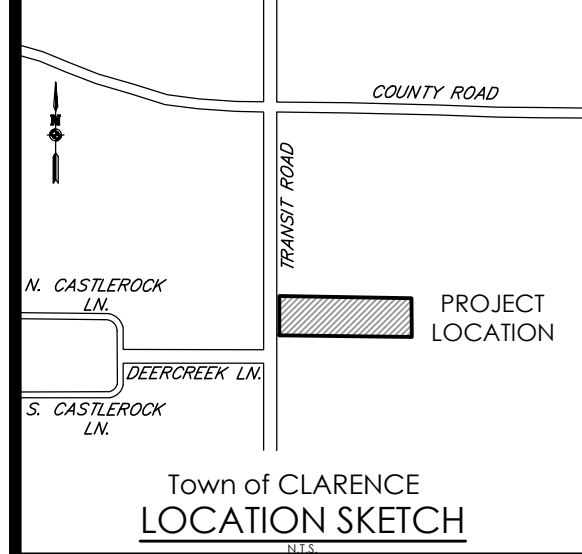
#### NOTES:

1. THE VOLUME OF THE NEW WATERMAIN SHALL BE DEDUCTED WHEN CALCULATING THE VOLUME OF SAND BACKFILL. COVER OVER NEW WATERMAINS SHALL BE 5' MINIMUM FROM PROPOSED FINISH GRADE.
2. SEE TRENCH EXCAVATION AND BACKFILL FOR ADDITIONAL NOTES, MATERIALS, PAY LIMITS AND DETAILS.



#### NOTES:

1. Lateral connection must be cored. Pneumatic devices or hammering knock-out methods are not acceptable for coring opening for new lateral pipe.
2. Size of cored opening shall be limited to one-half the nominal diameter of the mainline sewer.
3. Expose face of pipe from crown to invert for 12" along the length of the main. Core hole a minimum of 4" away from existing pipe hubs.
4. Take necessary precautions to prevent damage to the existing sewer, and to prevent debris from entering the sewer.
5. Thoroughly clean surface of existing pipe of foreign matter and loose particles prior to placement of tee connection.
6. Tee connection: Inserta Tee by Inserta or Fittings Co. or approved equal.
7. Lateral connections must be witnessed by a representative of the District. Notify Fire Warden a minimum of 48 hours in advance of construction for inspection of the lateral connection.



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(585) 325-1000  
Fax: (585) 325-1691  
Principal-in-Charge: Jess Sudol, PE  
Project Manager: David Cox, PE  
Designed by: James Ritzenhaler



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### DETAILS

### RWD CLARENCE

Town/City: CLARENCE	
County: ERIE	State: NEW YORK
Project No. <b>20213204.0001</b>	
Drawing No. <b>C 205</b>	Sheet No. <b>13</b>
Scale: <b>N.T.S.</b>	
Date <b>SEPTEMBER 2021</b>	

NOT FOR CONSTRUCTION



# NYSDOT STANDARD WORK ZONE TRAFFIC CONTROL NOTES

## 1. WORK ZONE TRAFFIC CONTROL REVISION

Proposed revisions to the Work Zone Traffic Control (WZTC) plan or modifications to the 619 Standard Sheets shall be submitted to the engineer for the review and approval by the Regional Traffic Engineer prior to the planned implementation of such revisions or modifications. The Contractor shall not implement the proposed revisions without approval from the Regional Traffic Engineer. When applicable, NYSDOT Work Zone Traffic Control (WZTC) typical applications shall be used. Typical can be found at <https://webapps.dot.ny.gov/work-zone-traffic-control>

## 2. TRAVEL LANE WIDTHS IN WORK ZONES

Where not shown in the WZTC plans or otherwise authorized by NYS DOT (or The Engineer), travel lane widths in work zones shall be a minimum of 11 ft on freeways, ramps, expressways and multi-lane\* conventional roadways and 10 ft on all other conventional roadways. (\*Multi-lane roadways are those with two or more travel lanes in one or both directions.

## 3. DAILY LANE, RAMP AND SHOULDER CLOSURE RESTRICTION

Work zones shall be restricted to one side of the roadway at a time in each direction on divided roadways, unless approved by the Engineer. The Contractor shall schedule work so that all travel lanes and ramps in each direction are open when the Contractor's operations are closed down or substantially closed down.

Daily closures may occur off of long-term closures and shall be subject to daily closure restrictions.

Work zones shall be restricted to one side of the roadway at a time on undivided highways.

There shall be no lane or shoulder closure during the PM Peak of 4-6 PM

## 4. SIGNALIZED INTERSECTIONS

When construction operations effect signalized intersections, the traffic signal shall be turned off before proceeding with the flagging operation, and any existing W3-3 signs covered.

The contractor shall return the signal to a 3-color operation at the end of the work operation. If the signal does not return to 3-color operation, the contractor shall notify the Region 3 Signal crew and must continue flagging until the Signal crew arrives and the signal is repaired.

The contractor shall obtain a police panel key from, and return it to, the Region 3 NYS DOT Signal crew EIC.

The NYS DOT Signal Crew EIC can be reached at 315-428-4064 or 315-426-2131.

## 5. HOLIDAY CLOSURE RESTRICTIONS

Daily lane, ramp and shoulder closures shall not be permitted on state owned roadways during major holidays. For a list of the major holidays, see special note in the contract proposal for temporary lane closure restrictions for major holidays.

Daily lane, ramp and shoulder closures shall not be permitted as described in E1 17-010. Below are holiday periods based on guidelines from E1 17-010. All restrictions are from 6:00 AM on the first day until 6:00 AM on the last day of the restriction:

## 2021

6:00 am Thursday, December 31, 2020 thru 6:00 am Monday, January 4, 2021 - (New Year's Holiday)

6:00 am Friday, May 28, 2021 thru 6:00 am Tuesday, June 1, 2021 - (Memorial Day Holiday)

6:00 am Friday, July 2, 2021 thru 6:00 am Tuesday, July 6, 2021 - (July 4<sup>th</sup> Holiday)

6:00 am Friday, September 3, 2021 thru 6:00 am Tuesday, September 7, 2021 - (Labor Day Holiday)

6:00 am Wednesday, November 24, 2021 thru 6:00 am Monday, November 29, 2021 - (Thanksgiving Holiday)

6:00 am Thursday, December 23, 2021 thru 6:00 am Monday, December 27, 2021 - (Christmas Holiday)

6:00 am Thursday, December 30, 2021 thru 6:00 am Monday, January 3, 2022 - (New Year's Holiday)

## 2022

6:00 am Thursday, December 30, 2021 thru 6:00 am Monday, January 3, 2022 - (New Year's Holiday)

6:00 am Friday, May 27, 2022 thru 6:00 am Tuesday, May 31, 2022 - (Memorial Day Holiday)

6:00 am Friday, July 1, 2022 thru 6:00 am Tuesday, July 5, 2022 - (July 4<sup>th</sup> Holiday)

6:00 am Friday, September 2, 2022 thru 6:00 am Tuesday, September 6, 2022 - (Labor Day Holiday)

6:00 am Wednesday, November 23, 2022 thru 6:00 am Monday, November 28, 2022 - (Thanksgiving Holiday)

6:00 am Friday, December 23, 2022 thru 6:00 am Tuesday, December 27, 2022 - (Christmas Holiday)

6:00 am Friday, December 30, 2022 thru 6:00 am Tuesday, January 3, 2023 - (New Year's Holiday)

## 2023

6:00 am Friday, December 30, 2022 thru 6:00 am Tuesday, January 3, 2023 - (New Year's Holiday)

6:00 am Friday, May 26, 2023 thru 6:00 am Tuesday, May 30, 2023 - (Memorial Day Holiday)

6:00 am Saturday, July 1, 2023 thru 6:00 am Wednesday, July 5, 2023 - (July 4<sup>th</sup> Holiday)

6:00 am Friday, September 1, 2023 thru 6:00 am Tuesday, September 5, 2023 - (Labor Day Holiday)

6:00 am Wednesday, November 22, 2023 thru 6:00 am Monday, November 27, 2023 - (Thanksgiving Holiday)

6:00 am Friday, December 22, 2023 thru 6:00 am Tuesday, December 26, 2024 - (Christmas Holiday)

6:00 am Friday, December 29, 2023 thru 6:00 am Tuesday, January 2, 2024 - (New Year's Holiday)

## 6. NOTIFICATION REQUIREMENTS

Region 3 has a Work Zone Traffic Control (WZTC) Notification Policy which requires Engineers/Contractor to notify the Regional Transportation Management Center (RTMC) prior to allowing a contractor to implement Work Zone Traffic Control activities within the highway right of way. Work Zone Notification is required for the following:

Freeways and Expressways: all lane, shoulder, road, ramp or bridge closures.

(This includes mobile operations which occupy the lane or shoulders.)

All Other State Highways: all lane closures whose duration will be greater than 2 hours and all road/bridge closures.

The Contractor shall report proposed WZTC activities noted above to the TMC by NOON of the business day (i.e. Monday through Friday excluding holidays) preceding the proposed WZTC activity. Failure to do so will result in disapproval to perform the unreported WZTC activity until the above notifications requirements are satisfied.

No planned WZTC activity shall be implemented without first receiving clearance from the RTMC.

## 7. VEHICLE RESTRICTIONS

The Contractor shall report any restriction (as defined below) on highways, ramps, or bridges to less than six (6) business weekdays in advance of the restriction. Six (6) days lead time is necessary to provide the RTMC adequate time to prevent issuance of Special Hauling Permits that would route oversize vehicles over the restricted section of this contract.

Restrictions shall be defined as one or more of the following:

- Complete closure of a highway, ramp or bridge.
- Installation of barrier or channelizing devices that result in an unobstructed width less than 18 feet along a highway, ramp or bridge.
- Suitable driving surfaces of less than 18 feet in width.
- Available vertical clearance above the highway is less than 14 feet in height.
- Work would limit vehicle length (i.e. turning ability)
- Changing the load capacity of a highway, ramp or bridge.

The Contractor shall also give verbal notification at least seven (7) business days (i.e. Monday through Friday excluding holidays) prior to and at the end of a restriction on any roadway to the:

## 8. WORK AREA COORDINATION

The Contractor shall coordinate work activities with other contractors within and/or adjacent to the contract work limits.

## 9. ACCESS

The Contractor shall ensure that active lanes of traffic on freeways are not crossed by pedestrian workers. For all other highways, the contractor shall ensure that pedestrian workers cross active lanes of traffic only at properly marked or unmarked crosswalks and/or dedicated pedestrian walkways. It is required that the Project Safety and Health Plan address access to each work and staging area.

Where it is feasible, vehicles and equipment used for the work and transporting of workers to/from the work site shall enter and leave the area closed by channelizing devices within the Termination Area of the Temporary Traffic Control Zone. Where such access within the Termination Area is not feasible, other areas for entry and exit shall be determined and included in the Project Safety & Health Plan, including illustrated examples (Typicals) to clearly show the temporary traffic control elements that will be provided.

## 10. CHANNELIZING DEVICES

All channelizing devices shall be placed so as to provide a 2 foot lateral clearance to the traveled way unless otherwise shown on the plans. Where possible a lateral buffer space of 2 foot minimum shall be provided between the work space and the channelizing devices.

Channelizing device spacing (center to center) shall be 40' maximum for posted speed limits 40 mph or greater and 20' maximum for posted speed limits 35 mph or less.

Standard cones and tubular markers shall not be used for channelization and delineation during the hours of darkness, which is defined as the period between sunset and sunrise.

## 11. SIGNS

All construction signs shall be mounted at a height of 7 feet above the edge of travel lane.

Signs shall not encroach more than 4" into shoulders of road by pedestrians or bicycles.

Where shoulder widths are limited and signs cannot be erected beyond the shoulder, construction signs may need to be mounted on concrete median barriers, bridge parapets, etc. and shall be determined and included in the Project Safety & Health Plan, including illustrated examples (Typicals) to clearly show the temporary traffic control elements that will be provided.

## 12. MISCELLANEOUS (local or Permit projects)

1. The Contractor shall be aware that the Work Zone Traffic Control is a very critical item of the permit and shall be provided in accordance with Section 619 "Work Zone Traffic Control" of the Standard Specifications, the 2009 edition of the National Manual on Uniform Traffic Control Devices for Streets and Highways and the New York State Supplement. The Contractor shall be responsible for Work Zone Traffic Control at all times for the duration of the permitted work.

2. Actual field conditions may require other signs and other arrangements of signs. Distances shall be adapted to prevailing conditions. Signs shall be located to provide optimum visibility. Signs that are not applicable shall be covered or obscured from sight. All sign numbers refer to the 2009 edition of the National Manual on Uniform Traffic Control Devices for Streets and Highways and the New York State Supplement.

3. Pedestrian accommodations shall be maintained for the duration of the proposed work. Any disturbed areas within the State Right-of-Way shall be adequately fenced to prevent pedestrian access when the contractors operations are shut down.

4. Materials, equipment and vehicles shall not be stored or parked within the State Right-of-Way before work begins or after contractor's operations are shut down. Staging areas outside the right-of-way shall be determined and included in the Project Safety & Health Plan, including illustrated examples (Typicals) to clearly show the temporary traffic control elements that will be provided.

5. Vehicles belonging to the Contractor or workers shall not be parked within 30 feet of the edge of pavement along a roadway being used by the general public, unless they are parked within a protected work area.

6. During non-working hours, construction equipment and materials shall not be stored within 30 feet of the edge of pavement.

7. W20-7A "Flagger" signs shall be used whenever flagging operations for more than a brief period of time. The signs shall be promptly removed, covered, or moved away from traffic when the flagging operation ceases.

All flagging stations and lane closures should be located to ensure maximum visibility.

8. No drop-off greater than six inches shall be left overnight within 30 feet of the edge of pavement. Drop-offs less than six inches will be permitted if proper delineation and signing is provided, and prior permission is granted in writing by a representative of the Department. A drop-off is considered eliminated if tapered away by a 1 on 6 slope or flatter.

9. Care shall be taken to insure that no damage occurs to the existing pavement/shoulder/curb areas as a result of construction equipment movement.

10. The Contractor may submit revisions to this plan for approval, but any change that alters the basic concepts of the plan must be approved by the NYSDOT Regional Director or his designee.

# NYSDOT STANDARD GENERAL PLAN NOTES

- ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
- ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES.
- MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE NEW YORK STATE RIGHT-OF-WAY.
- MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THE CURRENT NATIONAL MUTCD WITH NYS SUPPLEMENT, SECTION 619 OF THE CURRENT NYSDOT STANDARD SPECIFICATIONS, THESE PLANS AND AS ORDERED BY THE ASSISTANT RESIDENT ENGINEER, ON A NYSDOT CONSTRUCTION PROJECT. MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THESE PLANS AND BE IN ACCORDANCE WITH THE NYSDOT CONTRACT DOCUMENTS AS DEEMED NECESSARY BY THE NYS ENGINEER-IN-CHARGE.
- NOTIFY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION'S ASSISTANT RESIDENT ENGINEER FIVE (5) WORK DAYS PRIOR TO WORKING WITHIN THE STATE RIGHT-OF-WAY.
- NOTIFY THE NYSDOT SIGNAL MAINTENANCE FACILITY AT (585) 753-7793 5 DAYS PRIOR TO WORKING WITHIN 350' OF A SIGNALIZED INTERSECTION. NOTIFY DIG SAFELY NEW YORK 2 WORK DAYS PRIOR TO DIGGING, DRILLING OR BLASTING AT 811 FOR A UTILITY STAKE-OUT.
- ALL MATERIALS USED WITHIN THE STATE RIGHT-OF-WAY MUST COMPLY WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION'S STANDARD SHEETS.
- QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 401 OF THE STANDARD SPECIFICATIONS. ALL ASPHALT PRODUCED AS PART OF SECTION 401 WILL BE PAID AT A FINAL QUANTITY ADJUSTMENT FACTOR OF 1.0. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
- NO NIGHT WORK SHALL BE ALLOWED UNLESS APPROVED PRIOR TO START OF PROJECT. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC MAY BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
- HAZARDOUS WASTE NOTIFICATION: THE PERMITTEE ACCEPTS THE RIGHT-OF-WAY OF THE STATE HIGHWAY IN ITS "AS IS" CONDITION. THE DEPARTMENT OF TRANSPORTATION MAKES NO REPRESENTATION AS TO THE ABSENCE OF UNDERGROUND TANKS, STRUCTURES, FEATURES OR SIMILAR IMPEDIMENTS TO THE COMPLETION OF THE WORK PERMITTED HEREUNDER. SHOULD PERMITTEE FIND SOME PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS TO ITS WORK, THE DEPARTMENT OF TRANSPORTATION SHALL HAVE NO OBLIGATION TO CURE, REMOVE, REMEDY OR OTHERWISE DEAL WITH SUCH PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS. THE PERMITTEE IS REQUIRED TO REMOVE, MODIFY OR OTHERWISE DEAL WITH SUCH UNDERGROUND TANKS, STRUCTURES, FEATURES OR IMPEDIMENTS IN A MANNER WHICH MEETS ACCEPTABLE ENGINEERING PRACTICE AND IS APPROVED BY THE DEPARTMENT OF TRANSPORTATION.
- ADA COMPLIANCE - ALL WORK ON PEDESTRIAN FACILITIES SHALL BE COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT (ADA). 05/2018

## HIGHWAY RESTORATION DETAIL NOTES

- HMA ITEMS ARE BASED ON PG 44-22 BINDER. ESALS LESS THAN 30,000,000. TACK COAT IS REQUIRED BETWEEN ALL LIFTS OF ASPHALT. USE OF POLYPHOSPHORIC ACID (PPA) TO MODIFY THE PG BINDER PROPERTIES IS PROHIBITED. THIS PROHIBITION ALSO APPLIES TO THE USE OF PPA AS A CROSS-LINKING AGENT FOR POLYMER MODIFICATION.
- BUTT JOINTS, CREATED BY A FULL-DEPTH SAWCUT, SHALL BE USED BETWEEN NEW AND EXISTING PAVEMENTS. THE ASPHALT TOP COURSE SHALL BE LAID SUCH THAT IT UNIFORMLY OVERLAPS THE ADJACENT COLD MAT BY 2"-3". THE THICKNESS OF THE OVERLAP MATERIAL SHALL BE 1/4 THE COMPACTED THICKNESS OF THE COURSE, SO AS TO RESULT IN A SMOOTH AND WELL COMPACTED JOINT AFTER ROLLING. BROADCASTING OF THE OVERLAP MATERIAL ONTO THE LANE IS NOT ALLOWED. IF THE OVERLAP IS EXCESSIVE, THE EXCESS MATERIAL SHALL BE TRIMMED OFF SO THAT THE MATERIAL ALONG THE JOINT IS UNIFORM. THE COARSE PARTICLES OF AGGREGATE IN THE OVERLAP MATERIAL SHALL BE REMOVED AND WASTED IF DEEMED NECESSARY BY THE INSPECTOR. ASPHALT PAVEMENT JOINT ADHESIVE SHALL BE USED ON VERTICAL FACES FOR ALL OF THE JOINTS ON THE SURFACE COURSE SUCH AS LONGITUDINAL, TRANSVERSE AND OTHER JOINT BUTTING AGAINST THE NEW ASPHALT PAVEMENT.

## 3. DESIGN PGB CONTENT SELECTION

THE PRODUCER SELECTS THE DESIGN PGB CONTENT AT THE BINDER CONTENT THAT RESULTS IN A COMPACTED DENSITY OF 96.5% GAM AT THE DESIGN NUMBER OF GYRATIONS (DESIGN). UNDER NO CIRCUMSTANCES SHALL THE PERFORMANCE GRADED BINDER CONTENT IN THE HMA MIXTURE BE LESS THAN 5.8% FOR A 9.5 DESIGN, 5.2% FOR A 12.5 DESIGN, 4.5% FOR A 19.0 DESIGN, 4.2% FOR A 25.0 DESIGN, OR 3.7% FOR A 37.5 DESIGN. ALL VOLUMETRIC AND MECHANICAL PROPERTIES ARE CHECKED AT THIS PGB CONTENT TO ENSURE THAT ALL REQUIREMENTS ARE MET.

## 4. PERFORM ALL WARRANTY WORK IN ACCORDANCE WITH MATERIALS PROCEDURE (MP) 402-01, WARRANTY REQUIREMENTS FOR HOT MIX ASPHALT (HMA) TOP COURSE.

- AS PER STANDARD SPECIFICATION SECTION 401-4.01 CERTIFIED PRODUCTION, PRODUCTION LESS THAN 500 TONS, AND HIGHWAY PERMIT PRODUCTION. PRODUCTION MEETING THE SPECIFICATION REQUIREMENTS WILL BE ASSIGNED A QAF OF 1.00. PRODUCTION FAILING TO MEET THE SPECIFICATION REQUIREMENTS WILL BE SUBJECT TO EVALUATION ACCORDING TO SECTION 401-4.03. EVALUATION OF SUBLOTS REPRESENTED BY 0.85 QAF.

# NYSDOT UTILITY PERMIT NOTES

\* ROAD PAVEMENT, SHOULDER AND DRAINAGE ARE NOT TO BE DISTURBED OR UNDERMINED UNLESS INDICATED OTHERWISE IN THE PLANS.

\* SUITABLE NON-FROZEN, NON SATURATED BACKFILL TO BE USED AND PLACED IN 6" COMPACTED LIFTS.

\* PAVEMENT TO BE MAINTAINED UNTIL A PERMANENT REPAIR CAN BE MADE.

\* PAVEMENT TO BE REPLACED IN KIND OR AS SPECIFIED ON ATTACHED PLANS.

\* ALL DISTURBED GRASS AREAS WITHIN THE R.O.W. ARE TO BE FINE GRADED, 4" TOPSOIL PLACED AND TURF ESTABLISHED.

## NOTE:

ROAD PLATES SHALL NOT BE USED DURING THE SNOW AND ICE SEASON® (NOVEMBER 1ST THROUGH APRIL 1ST), DURING THE MONTHS OF OCTOBER AND APRIL, CALL THE WORK AREA JURISDICTIONAL N.Y.S.D.O.T. TRANSPORTATION MAINTENANCE RESIDENCY WITH PLATE LOCATIONS. ROAD PLATES SHALL BE PINNED AND RAMPED. A "STEEL PLATE AHEAD" SIGN (WB-24) SHALL BE INSTALLED IN ADVANCE OF THE PLATE IN ACCORDANCE WITH TABLE NY4H-3 ON STANDARD SHEET 619-11. IF THE SURFACE OF THE PLATE IS 1" OR GREATER ABOVE/BELOW THE SURROUNDING PAVEMENT, A "BUMP" SIGN (WB-1) SHALL BE INSTALLED BETWEEN THE PLATE AND "STEEL PLATE AHEAD" SIGN IN ACCORDANCE WITH TABLE NY4H-3 ON STANDARD SHEET 619-11 AND A BARREL WITH A TYPE A FLASHER SHALL BE PLACED ON THE SHOULDER AT THE PLATE.

## WINTER ASPHALT PAVEMENT AND SHOULDER RESTORATION:

EXCAVATION BACKFILL SHALL BE WITH NO. 2 STONE (NOT CRUSHER RUN), ITEM 623.12, PLACED TO SIX INCHES (6") BELOW THE EXISTING PAVEMENT SURFACE. THE REMAINING SIX INCHES (6") SHALL HAVE CONCRETE PLACED TO MATCH EXISTING ADJACENT PAVEMENT GRADE AND SHALL HAVE A GOOD, SMOOTH, NON-POLISHED RIDING SURFACE.

IN THE SPRING, WHEN ASPHALT PLANTS OPEN, THE CONCRETE AND STONE SHALL BE REMOVED TO THE EXISTING BOTTOM OF THE SUBBASE IN ORDER FOR PERM FIRM FIN RESTORATION PER PREVIOUSLY APPROVED N.Y.S.D.O.T. PAVEMENT RESTORATION PROCEDURES.

## TEST PITS:

ALL TEST PITS IN THE PAVEMENT SHALL BE COMPLETED BY AN 18" SQUARE/ROUND VACUUM EXCAVATION.

## EXCAVATION SUPPORT SYSTEM REQUIREMENTS:

ANY EXCAVATION 5 FEET OR MORE IN DEPTH SHALL UTILIZE A SHIELDS AND SHORING OR SHEETING SYSTEM WHICH PROVIDES DIRECT CONTACT AND SUPPORT OF THE EXCAVATION SIDES. THE SYSTEM SHALL ACCOMMODATE ANY ASSOCIATED SURCHARGE LOADS AND SHALL BE SUBMITTED TO NYSDOT'S REGIONAL GEOTECHNICAL ENGINEER FOR REVIEW AND APPROVAL PRIOR TO PERMIT ISSUANCE.

A SLOPING (LAYBACK) OPTION WILL NOT BE ALLOWED.

SHIELDS AND SHORING OR SHEETING SYSTEM REQUIREMENTS DO NOT PERTAIN TO TEST PITS WHICH ARE 18" OR LESS IN DIAMETER.

R-4 (03/15)

## ALL ROADWAYS

## Notes:

- The ROAD WORK AHEAD Sign may be replaced with other appropriate signs such as the SHOULDER WORK sign. The SHOULDER WORK sign may be used for work adjacent to the shoulder.
- The ROAD WORK AHEAD sign may be omitted where: The work space is behind a barrier or guide rail, or more than 2 FT behind the curb or 15 FT or more from the edge of any roadway.
- In urban conditions, advance warning sign spacing may be reduced to 100 FT. (Min.) in order to accommodate side streets and driveways.



The END ROAD WORK sign shall be located a maximum distance of 500' (12 Skip Lines) past the work area.

**END ROAD WORK**  
G20-2  
36x18 in. (Conventional Roadway)  
48x24 in. (Freeway/Expressway)

**ROAD WORK AHEAD**  
W20-1  
36x36 in. (Conventional Roadway)  
48x48 in. (Freeway/Expressway)

**SHOULDER WORK**  
W21-5  
36x36 in. (Conventional Roadway)  
48x48 in. (Freeway/Expressway)

**Work Area**

**NYSDOT WORK ZONE TRAFFIC CONTROL**

**ALL DURATION OPERATION INVOLVING WORK BEYOND THE SHOULDER**

**ALL ROADWAY TYPES**

**OCTOBER 2008 TAG-CE1**

## TAG-CE1

## END ROAD WORK

END ROAD WORK Sign is Optional If work duration is less than 1 Hour

Urban Low Speed (30-40 mph): 200 FT. (5 Skip Lines) See Note 3

Urban High Speed (45-55 mph): 350 FT. (~9 Skip Lines) See Note 3

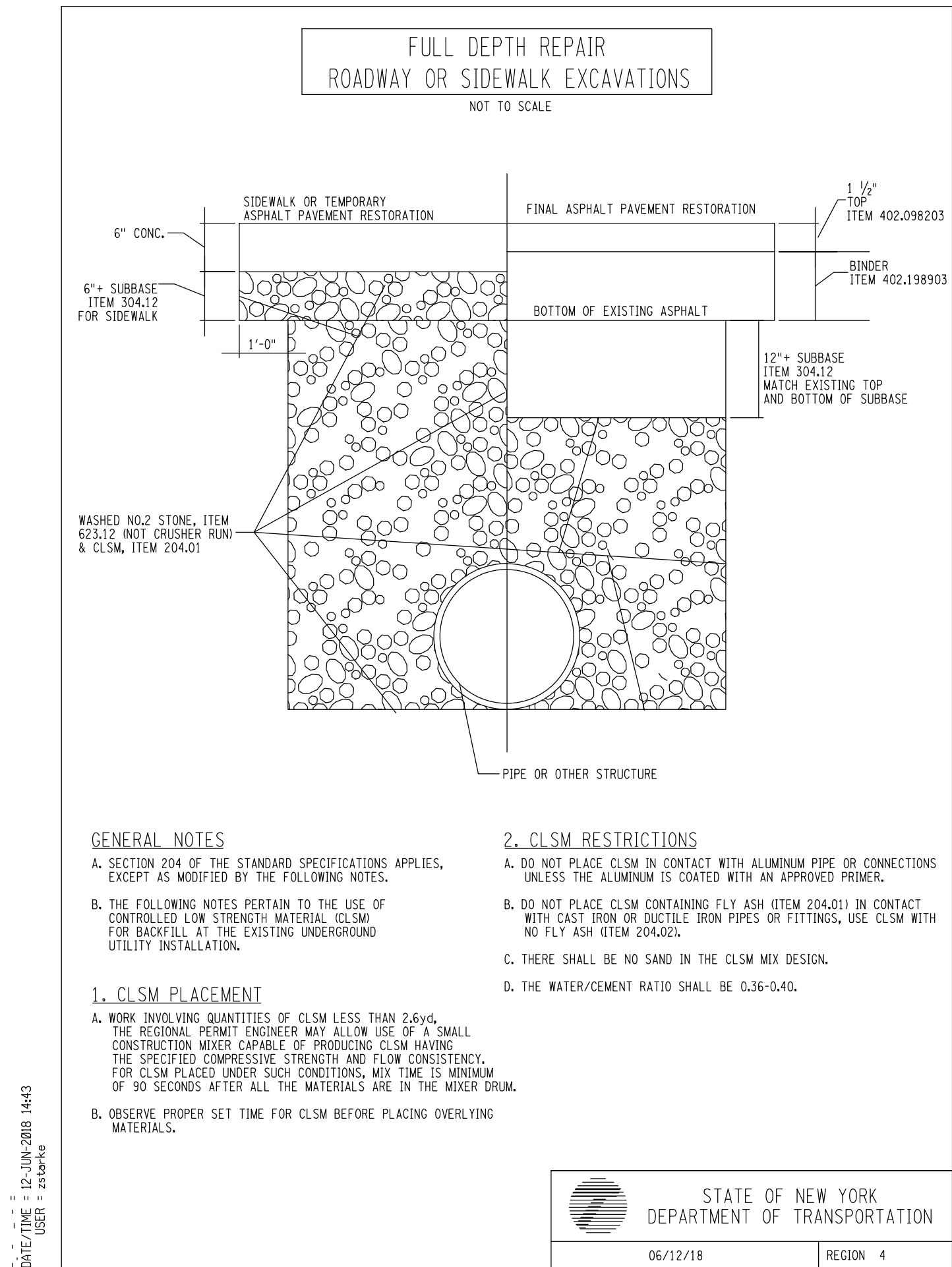
Rural Roadway: 500 FT. (~13 Skip Lines/1 Reference Marker)

Freeway/Expressway: 1000 FT. (~26 Skip Lines/2 Reference Markers)

See Note 2

See Note 1

NOT TO SCALE



## GENERAL NOTES

A. SECTION 204 OF THE STANDARD SPECIFICATIONS APPLIES, EXCEPT AS MODIFIED BY THE FOLLOWING NOTES.

B. THE FOLLOWING NOTES PERTAIN TO THE USE OF CONTROLLED LOW STRENGTH MATERIAL (CLSM) FOR BACKFILL AT THE EXISTING UNDERGROUND UTILITY INSTALLATION.

## 1. CLSM PLACEMENT

A. WORK INVOLVING QUANTITIES OF CLSM LESS THAN 2,600 yd<sup>3</sup>. THE REGIONAL PERMIT ENGINEER MAY ALLOW USE OF A SMALL CONSTRUCTION MIXER CAPABLE OF PRODUCING CLSM HAVING THE SPECIFIED COMPRESSIVE STRENGTH AND FLOW CONSISTENCY. FOR CLSM PLACED UNDER SUCH CONDITIONS, MIX TIME IS MINIMUM OF 90 SECONDS AFTER ALL THE MATERIALS ARE IN THE MIXER DRUM.

B. OBSERVE PROPER SET TIME FOR CLSM BEFORE PLACING OVERLYING MATERIALS.

## 2. CLSM RESTRICTIONS

A. DO NOT PLACE CLSM IN CONTACT WITH ALUMINUM PIPE OR CONNECTIONS UNLESS THE ALUMINUM IS COATED WITH AN APPROVED PRIMER.

B. DO NOT PLACE CLSM CONTAINING FLY ASH (ITEM 204.01) IN CONTACT WITH CAST IRON OR DUCTILE IRON PIPES OR FITTINGS. USE CLSM WITH NO FLY ASH (ITEM 204.02).

C. THERE SHALL BE NO SAND IN THE CLSM MIX DESIGN.

D. THE WATER/CEMENT RATIO SHALL BE 0.36-0.40.



STATE OF NEW YORK  
DEPARTMENT OF TRANSPORTATION

06/12/18

REGION 4

**PA**  
PASSERO ASSOCIATES  
engineering architecture

1/2" = 1'-0"

1/4" = 1'-0"

1/8" = 1'-0"

1/16" = 1'-0"

1/32" = 1'-0"

1/64" = 1'-0"

1/128" = 1'-0"

1/256" = 1'-0"

1/512" = 1'-0"

1/1024" = 1'-0"

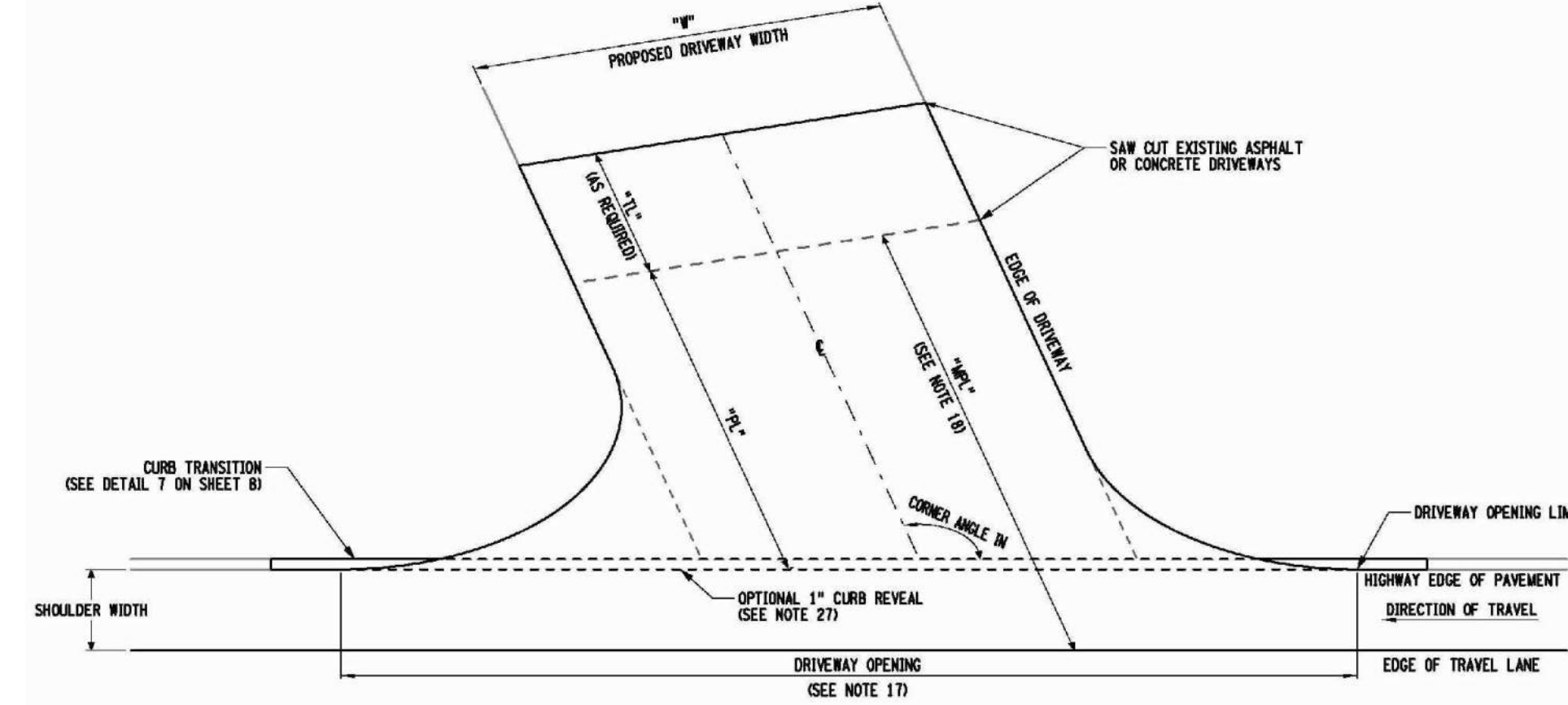
1/2048" = 1'-0"

1/4096" = 1'-0"

1/8192" = 1'-0"

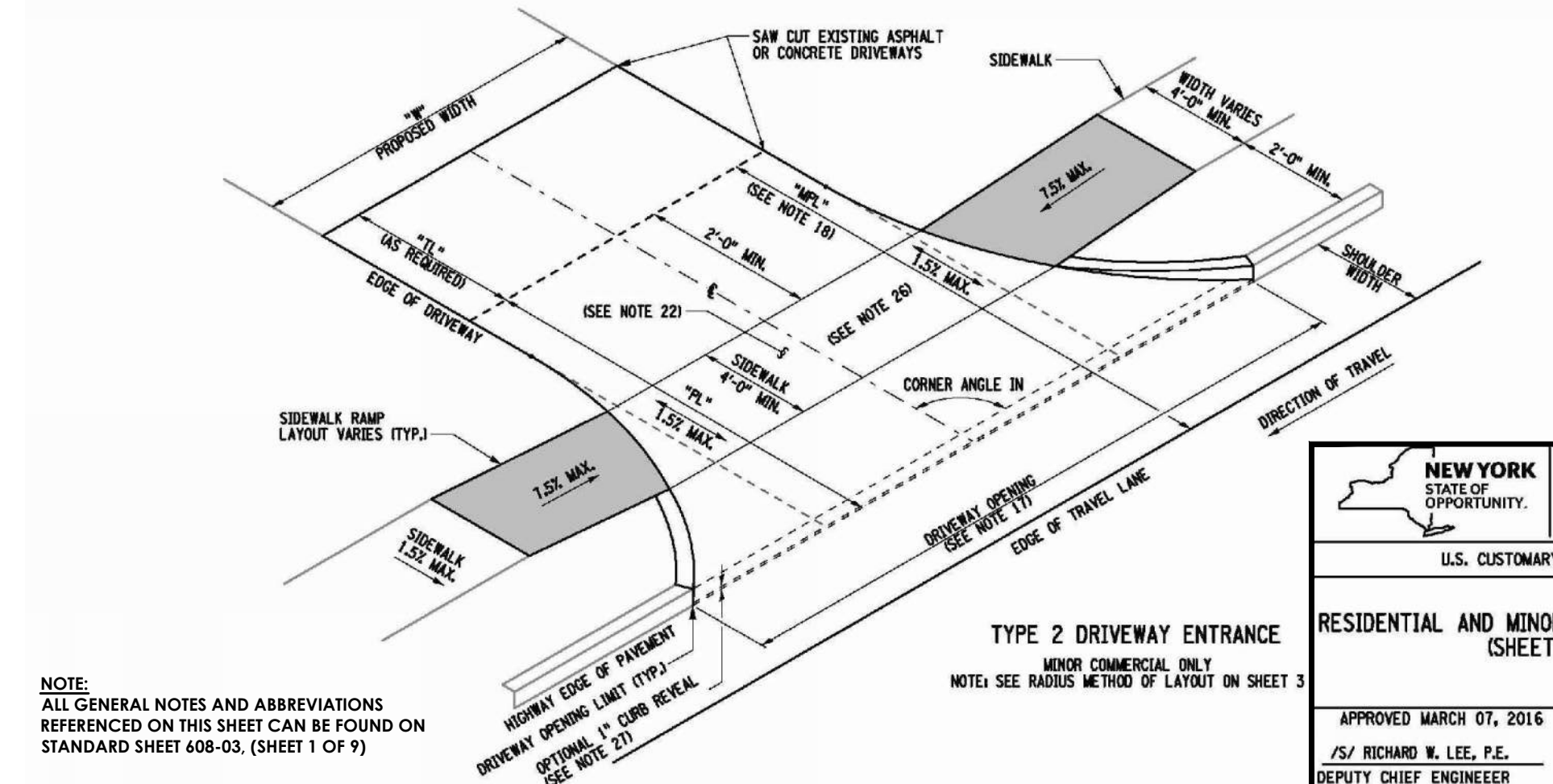
1/16384" = 1'-0





TYPE 1 DRIVEWAY ENTRANCE

NOTE: SEE RADIUS METHOD OF LAYOUT ON SHEET 3



TYPE 2 DRIVEWAY ENTRANCE

MINOR COMMERCIAL ONLY  
NOTE: SEE RADIUS METHOD OF LAYOUT ON SHEET 3

#### DEFINITION OF TERMS

DRIVEWAY - EVERY ENTRANCE OR EXIT USED BY VEHICULAR TRAFFIC TO AND FROM LANDS OR BUILDINGS ADJACENT TO A HIGHWAY.

RESIDENTIAL DRIVEWAY - A DRIVEWAY SERVING FOUR OR FEWER PRIVATE HOMES OR AN APARTMENT BUILDING FOR FOUR OR FEWER FAMILY UNITS.

COMMERCIAL DRIVEWAY - A DRIVEWAY SERVING A COMMERCIAL ESTABLISHMENT, INDUSTRY, GOVERNMENTAL, OR EDUCATIONAL INSTITUTION, PRIVATE UTILITY, HOSPITAL, CHURCH, APARTMENT BUILDING, OR OTHER COMPARABLE TRAFFIC GENERATOR.

MAJOR COMMERCIAL DRIVEWAY - ANY COMMERCIAL DRIVEWAY WHERE THE ACTUAL OR ANTICIPATED TRAFFIC VOLUME ON A TYPICAL DAY IS DEFINED BY THE DRIVEWAY POLICY AS DEFINED IN THE HIGHWAY DESIGN MANUAL, NOW CHAPTER 5, APPENDIX 5A.

MINOR COMMERCIAL DRIVEWAY - ANY COMMERCIAL DRIVEWAY WHERE THE ACTUAL OR ANTICIPATED TRAFFIC VOLUMES ON A TYPICAL DAY ARE LESS THAN THE VALUES STIPULATED FOR A MAJOR COMMERCIAL DRIVEWAY.

FIELD ENTRANCE - A DRIVEWAY SERVING A FARMYARD, CULTIVATED OR UNCULTIVATED FIELD, TIMBERLAND, OR UNDEVELOPED LAND NOT USED FOR INDUSTRIAL, COMMERCIAL, OR RESIDENTIAL PURPOSES.

URBAN / RURAL - THE AREA CHARACTER BASED ON NYSDOT HIGHWAY DESIGN MANUAL CHAPTER 2, SECTION 2.4.

DRIVEWAY OFFSET - THE DISTANCE IN FEET MEASURED FROM THE INSIDE EDGE OF THE OUTERMOST TRAVEL LANE, OR TURNING LANE, TO THE HIGHWAY EDGE OF PAVEMENT. THE DISTANCE IS EQUAL TO THE WIDTH OF THE OUTERMOST LANE AND THE WIDTH OF THE PAVED SHOULDER, OR CURB OFFSET.

HIGHWAY EDGE OF PAVEMENT - THE OUTSIDE EDGE OF THE PAVED HIGHWAY SURFACE.

SHOULDER WIDTH - THE WIDTH IN FEET OF PAVED SHOULDER INCLUDING A PARKING LANE, BIKE LANE, CURB OFFSET, OR OTHER PAVED AREA OUTSIDE OF THE TRAVEL LANE.

MINIMUM PAVING LIMIT (MPL) - THE MINIMUM DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY FROM THE OUTSIDE EDGE OF THE OUTERMOST TRAVEL LANE THAT A DRIVEWAY MUST BE PAVED INCLUDING THE SHOULDER WIDTH.

PAVEMENT LENGTH (PL) - THE DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY FROM THE HIGHWAY EDGE OF PAVEMENT TO THE END OF PROPOSED DRIVEWAY PAVEMENT.

TRANSITION LENGTH (TL) - THE DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY BEYOND THE DRIVEWAY PAVEMENT LENGTH (PL) TO THE END OF PROPOSED DRIVEWAY WORK. THE TRANSITION LENGTH (TL) IS TYPICALLY USED FOR GRADING, LAYOUT, OR TRANSITION REASONS. THE TRANSITION LENGTH (TL) ONLY APPLIES TO DRIVEWAYS THAT ARE UNPAVED.

BUFFER ZONE - A PHYSICAL DISTANCE SEPARATING THE PEDESTRIAN ACCESS ROUTE AND THE VEHICLE TRAVELED WAY. THE BUFFER ZONE BUFFERS PEDESTRIANS FROM TRAFFIC AND PROVIDES SPACE FOR SNOW STORAGE, UTILITIES, PLANTS, AND OTHER STREET APPURTENANCES. THE BUFFER ZONE MAY BE PLANTED OR PAVED.

SHARED-USE-PATH (SUP) - A BICYCLE AND PEDESTRIAN FACILITY, TYPICALLY WITHIN THE RIGHT-OF-WAY, SEPARATED FROM MOTORIZED VEHICULAR TRAFFIC BY A BUFFER ZONE OR BARRIERS. REFER TO HIGHWAY DESIGN MANUAL, CHAPTER 11 AND AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES FOR GUIDANCE ON BUFFER ZONE WIDTH AND SEPARATION OF SHARED USE PATHS FROM ROADWAYS.

SIDEWALK - A SMOOTH, STABLE AND SLIP RESISTANT EXTERIOR PATHWAY INTENDED FOR PEDESTRIAN USE ALONG A VEHICULAR WAY SEPARATED WITH A CURB OFFSET.

HMA - HOT MIX ASPHALT

POC - PORTLAND CEMENT CONCRETE

#### GENERAL NOTES FOR DRIVEWAY STANDARD SHEETS

- THE DRIVEWAY STANDARD SHEETS APPLY TO FIELD ENTRANCES, RESIDENTIAL DRIVEWAYS AND MINOR COMMERCIAL DRIVEWAYS. FIELD ENTRANCES AND RESIDENTIAL DRIVEWAYS ACCOMMODATE AN AASHTO PASSENGER CAR DESIGN VEHICLE. MINOR COMMERCIAL DRIVEWAYS ACCOMMODATE AN AASHTO SINGLE UNIT TRUCK DESIGN VEHICLE.
- DRIVEWAY WORK PERFORMED OFF THE RIGHT-OF-WAY REQUIRES AN EASEMENT OR A DRIVEWAY RELEASE. A DRIVEWAY RELOCATION WILL REQUIRE A TEMPORARY EASEMENT MAP.
- IF COMMERCIAL PROPERTY DEVELOPMENT PLANS INVOLVE NEW OR MODIFIED ACCESS TO A STATE HIGHWAY A COMMERCIAL HIGHWAY WORK PERMIT APPLICATION FORM PERM 35-COMM MUST BE FILLED OUT AND SUBMITTED TO THE REGIONAL PERMIT COORDINATOR.
- SEE THE DRIVEWAY TABLE IN THE CONTRACT PLANS FOR SPECIFIC DRIVEWAY LOCATIONS, WIDTHS ("W"), CORNER ANGLES, LENGTHS ("L"), MATERIAL, AND ENTRANCE TYPE.
- DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAY APRONS.
- THE TAPER METHOD IS GENERALLY NOT RECOMMENDED FOR DRIVEWAYS WITH A DRIVEWAY OFFSET LESS THAN 16 FEET, UNLESS IT CAN BE FIELD VERIFIED THAT THE DRIVEWAY ENTRANCE WIDTH WILL ACCOMMODATE THE VEHICLES THAT USE THE DRIVEWAY ON A REGULAR BASIS.
- TYPE 3 AND TYPE 4 DRIVEWAY ENTRANCES CAN BE USED WITHOUT CURB IF A TAPER STYLE ENTRANCE BETTER MATCHES THE HIGHWAY CORRIDOR AESTHETICS OR SPECIFIC SITE CONDITIONS THAN A RADIUS STYLE ENTRANCE.
- UP TO 10° OF HMA MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.
- UP TO 9° OF POC MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.
- UP TO 12° OF SUBBASE MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.
- THE DETAILS SHOW THE PAVEMENT LENGTH ("PL") EXTENDING TO THE MINIMUM PAVING LIMIT ("MPL"). HOWEVER, THE "MPL" CAN EXTEND BEYOND THE "MPL" AS SPECIFIED IN THE CONTRACT DOCUMENTS.
- A DRIVEWAY TIP-UP SECTION SHOULD EXTEND TO A LOGICAL TERMINUS (EXAMPLE: SIDEWALK EDGE, WHERE THE DRIVEWAY GRADE MATCHES EXISTING GROUND, OR LAYOUT POINT). FOR REFERENCE, A REASONABLE LENGTH FOR TAPERING THE TIP-UP SECTION BACK TO THE EDGE OF DRIVEWAY IS 3 TO 4 TIMES THE LENGTH OF CURB DROP. THE TIP-UP SECTION IS NOT PART OF THE DRIVEWAY OPENING WIDTH. REFER TO NYSDOT STANDARD SHEET 609-02 "MISCELLANEOUS CURB DETAILS" FOR THE CURB TRANSITION.
- TO DETERMINE THE LIMITS OF SHOULDER RECONSTRUCTION, REFER TO THE DRIVEWAY OPENING TABLES ON SHEET 4 FOR NO SHOULDER OR OFFSET.
- FOR POC SHOULDER, SEE STANDARD SHEET 502-02 FOR LONGITUDINAL JOINT DETAILS.
- DIMENSIONS AND ANGLES MAY BE INTERPOLATED FOR VALUES OTHER THAN THOSE SHOWN IN THE TABLES.
- THE SHOULDER PAVEMENT THICKNESSES SHOWN ARE DEFAULT VALUES UNLESS OTHERWISE SHOWN IN THE PLANS. MATERIALS SHALL BE AS SPECIFIED IN THE CONTRACT DOCUMENTS.

#### WIDTH / LENGTH

- WHERE THERE ARE CONSTRAINTS THAT PREVENT THE CONSTRUCTION OF THE DRIVEWAY OPENING USING EITHER OF THE LAYOUT METHODS, THE ENGINEER MAY SPECIFY A SMALL CORNER CURB RADIUS OF 2' OR A 1/2" BULL HORN CURB ALONG LOW SPEED HIGHWAYS, PROVIDED THE DRIVEWAY OPENING MEETS THE REQUIREMENTS OF THE "DRIVEWAY OPENING" TABLES ON SHEET 4.
- FOR RESIDENTIAL DRIVEWAYS, THE MINIMUM PAVING LIMIT SHALL BE 10' FROM THE OUTSIDE EDGE OF TRAVEL LANE OR 2' BEHIND ANY SIDEWALK, IF PRESENT, WHICHEVER IS GREATER. FOR MINOR COMMERCIAL DRIVEWAYS, THE MINIMUM PAVING LIMIT SHALL BE 30' FROM THE OUTSIDE EDGE OF TRAVEL LANE OR 2' BEHIND ANY SIDEWALK, IF PRESENT, OR EXTEND TO THE RIGHT-OF-WAY LINE, WHICHEVER IS GREATER. THE PAVING LIMIT MAY EXTEND BEYOND THE MINIMUM PAVING LIMIT FOR NEW DRIVEWAYS AND TO TRANSITION TO EXISTING PAVED DRIVEWAYS. THE PAVING LIMIT WILL BE NOTED IN THE DRIVEWAY TABLE OF THE CONTRACT PLANS.
- FOR GRADING AND CONSTRUCTION REQUIREMENTS OF TRANSITIONS FROM PLACED HMA TO EXISTING HMA DRIVEWAYS, REFER TO DETAIL 9 - "TIE-IN TO EXISTING DRIVEWAYS" ON SHEET 9, AND TABLE 3 - "DRIVEWAY MATERIALS AND THICKNESS" ON SHEET 2.
- FOR POC DRIVEWAYS, REFER TO THE 502 SERIES STANDARD SHEETS FOR METAL REINFORCEMENT, JOINT TIES, SAWING AND SEALING, ETC.
- A 5' MINIMUM BUFFER ZONE SHALL BE USED UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

#### SITE CONDITIONS (SIDEWALK / CURB)

- ANY POC SIDEWALK WHICH CROSSES A DRIVEWAY SHALL HAVE A MINIMUM THICKNESS OF 6" AND INCLUDE STEEL MESH REINFORCEMENT WITH 3" OF TOP COVER.
- FOR GRADE CHANGES REFER TO THE DRIVEWAY PROFILES ON SHEET 8. VERTICAL CURVES ARE RECOMMENDED TO CONNECT TANGENTS. SEE TABLE 5 - "MINIMUM LENGTH OF VERTICAL CURVE" ON SHEET 2 FOR TYPICAL VERTICAL CURVE LENGTHS "L".
- WHERE THE EXISTING GRADE OF THE DRIVEWAY PROFILE IS LESS THAN OR EQUAL TO 2%, MATCH THE CROSS SLOPE OF THE SIDEWALK TO THE EXISTING DRIVEWAY PROFILE GRADE.
- WHERE THE EXISTING GRADE OF THE DRIVEWAY PROFILE EXCEEDS 2%, SLOPE THE DRIVEWAY AND RECONSTRUCT A MINIMUM OF 2' ON BOTH SIDES OF THE SIDEWALK, TO TRANSITION FROM THE EXISTING GRADE OF THE DRIVEWAY PROFILE TO THE SIDEWALK CROSS SLOPE.
- TO PREVENT DRIVEWAY GRADES FROM EXCEEDING THE VALUES IN TABLE 2 - "MAXIMUM DRIVEWAY SLOPE" ON SHEET 2, IT MAY BE NECESSARY TO DEPRESS THE SIDEWALK ACROSS THE DRIVEWAY. SIDEWALK RAMPS SHALL HAVE THE LEAST RUNNING SLOPE POSSIBLE WITH A MAXIMUM DESIGN AND LAYOUT SLOPE OF 7.5%. THE RUNNING SLOPE FOR WORK ACCEPTANCE SHALL BE A MAXIMUM OF 0.3% WHERE EXISTING CONDITIONS DO NOT ALLOW THE CONSTRUCTION OF A SIDEWALK RAMP AT 0.3% OR LESS RUNNING SLOPE. THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-0" FOR DESIGN AND LAYOUT. THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-0" FOR WORK ACCEPTANCE.
- WHERE DRAINAGE IS CARRIED ALONG THE CURB, CONSTRUCT THE DRIVEWAY WITH A SHORT UPSLOPE TO PREVENT RUNOFF FROM POOLING AT THE DRIVEWAY ENTRANCE. FLAT DRIVEWAY OR RUNNING DOWN THE DRIVEWAY DOWNHILL DRIVEWAY SLOPE, IF CONDITIONS MAKE THE ADDITION OF A SHORT UPSLOPE IMPRACTICAL, USE 1" CURB REVEAL AND CONTINUE CURB ACROSS THE DRIVEWAY OPENING. TYPICALLY, CURB REVEAL WILL NOT BE CONSTRUCTED IN RURAL AREAS. IF CURB REVEAL IS SPECIFIED FOR A SPECIFIC DRIVEWAY, IT WILL BE NOTED IN THE DRIVEWAY TABLE OF THE CONTRACT PLANS IN THE "COMMENTS" COLUMN.

#### ENTRANCE TYPE

- THE ENGINEER MAY INTERCHANGE TYPE 1, TYPE 3 AND TYPE 4 RESIDENTIAL DRIVEWAYS TO BETTER MATCH THE EXISTING ENTRANCE TYPES ALONG THE HIGHWAY CORRIDOR WHILE CONSIDERING AVAILABLE SPACE, CONSTRUCTABILITY, SAFETY, AND FUNCTIONALITY. THE DRIVEWAY TYPE SHALL COMPLY WITH TABLE 4 - "DRIVEWAY ENTRANCE TYPE SELECTION" ON SHEET 2.
- FOR DRIVEWAYS WITH VARYING WIDTHS AND/OR CURVED ALIGNMENTS, DETERMINE THE DRIVEWAY WIDTH AND CORNER ANGLE 20'-0" FROM THE EDGE OF TRAVEL LANE.
- FOR A ONE-WAY DRIVEWAY ENTRANCE OR EXIT, THE DRIVEWAY ENTRANCE WIDENING IS ONLY NECESSARY ON ONE SIDE OF THE DRIVEWAY TO ACCOMMODATE THE SHARPER TURNING MOVEMENT. ONE-WAY DRIVEWAYS WILL BE IDENTIFIED ON THE DRIVEWAY TABLE OF THE CONTRACT PLANS UNDER "COMMENTS" FOR CURVED HIGHWAYS. A SMALL CORNER CURB RADIUS OF 2' ON 1/2" BULLHORN CURB ALONG LOW SPEED HIGHWAYS SHALL BE CONSTRUCTED TO ELIMINATE A SHARP CORNER BEND IN THE CURB LINE WHICH IS SAFER FOR SNOWPLOW OPERATIONS.

#### MATERIAL

- FOR DRIVEWAY MATERIAL REQUIREMENTS, USE TABLE 3 - "DRIVEWAY MATERIALS AND THICKNESS" ON SHEET 2.
- FOR FIELD ENTRANCES, THE MATERIAL WITHIN THE PAVEMENT LENGTH ("PL") CAN CONSIST OF GRAVEL OR STONE AND BE CONNECTED TO THE EDGE OF THE HIGHWAY SHOULDER WITHOUT REMOVING ANY OF THE EXISTING SHOULDER MATERIAL.

**NEW YORK STATE OF OPPORTUNITY.**

**Department of Transportation**

U.S. CUSTOMARY STANDARD SHEET

**RESIDENTIAL AND MINOR COMMERCIAL DRIVEWAYS (SHEET 5 OF 9)**

APPROVED MARCH 07, 2016

/S/ RICHARD W. LEE, P.E.  
DEPUTY CHIEF ENGINEER (DESIGN)

ISSUED UNDER EB 16-012

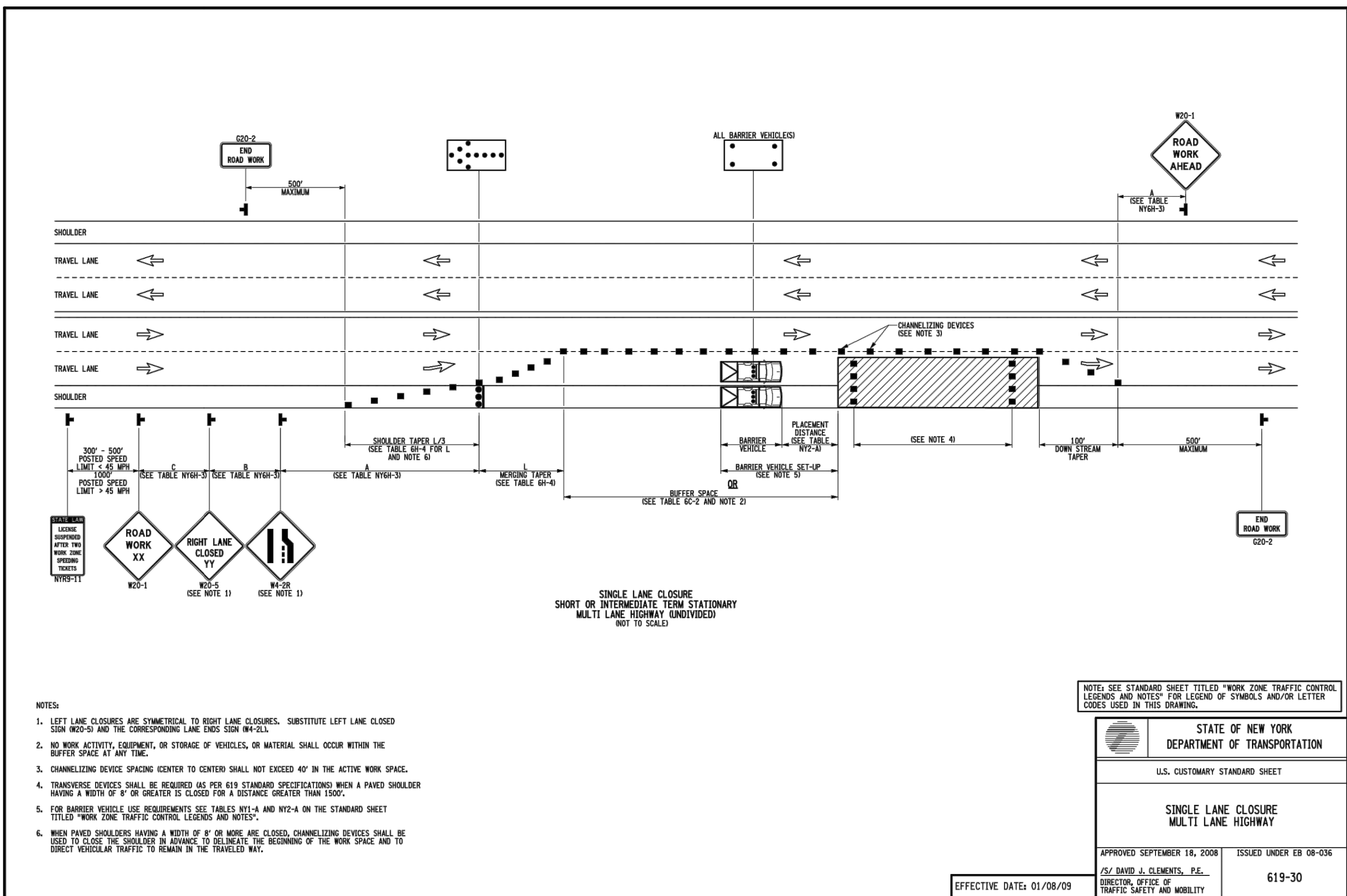
608-03

DESIGN ELEMENT TOLERANCES		
ELEMENT	DESIGN AND FIELD LAYOUT LIMIT	LIMIT FOR WORK ACCEPTANCE
SIDEWALK CROSS SLOPE - SEE NOTE 12	1.5% MAX.	2.0% MAX.
SIDEWALK GRADE (RUNNING SLOPE) - SEE NOTE 11	4.5% MAX.	5.0% MAX.
CURB RAMP GRADE (RUNNING SLOPE) - SEE NOTE 21	7.5% MAX.	8.3% MAX.
BLENDED TRANSITION GRADE (RUNNING SLOPE) - SEE NOTE 7	4.5% MAX.	5.0% MAX.

NOTES REFERENCED IN THE TABLE ABOVE CAN BE FOUND ON STANDARD SHEET 608-01 SHEET 1 OF 9.

ALL VALUES SHOWN ON THE 608-03 STANDARD SHEETS REFER TO DESIGN AND FIELD LAYOUT LIMITS.

FOR ADDITIONAL REQUIREMENTS AND TOLERANCES, SEE "CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT, AND CONSTRUCTION OF PEDESTRIAN FACILITIES" AVAILABLE ON THE NYSDOT HIGHWAY DESIGN MANUAL CHAPTER 18 WEBSITE.



**STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION**

U.S. CUSTOMARY STANDARD SHEET

**SINGLE LANE CLOSURE MULTI LANE HIGHWAY**

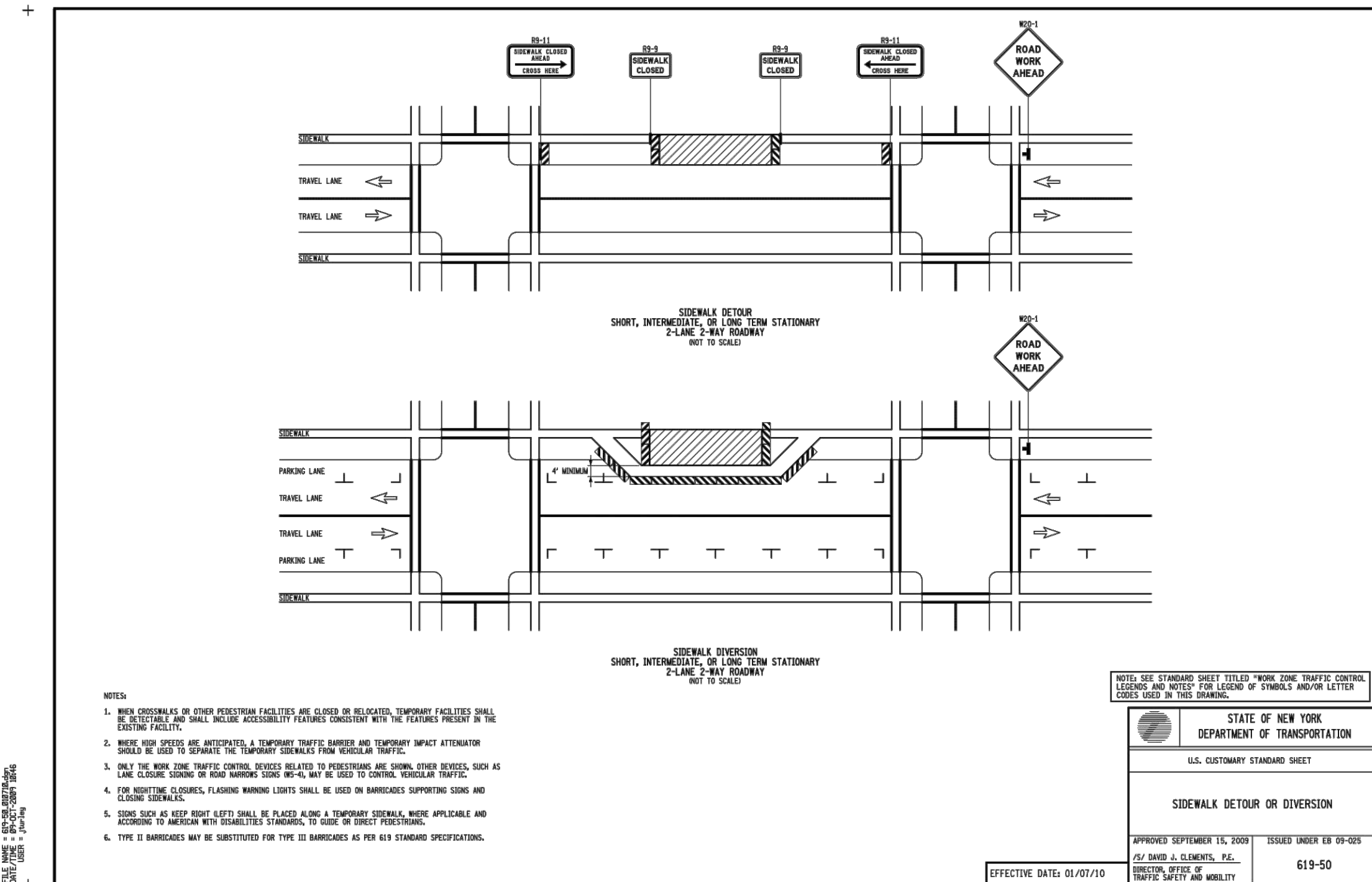
APPROVED SEPTEMBER 18, 2008

/S/ DAVID J. CLEMENTS, P.E.  
DIRECTOR, OFFICE OF TRAFFIC SAFETY AND MOBILITY

ISSUED UNDER EB 08-036

619-30

EFFECTIVE DATE: 01/08/09



**STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION**

U.S. CUSTOMARY STANDARD SHEET

**SIDEWALK DETOUR OR DIVERSION**

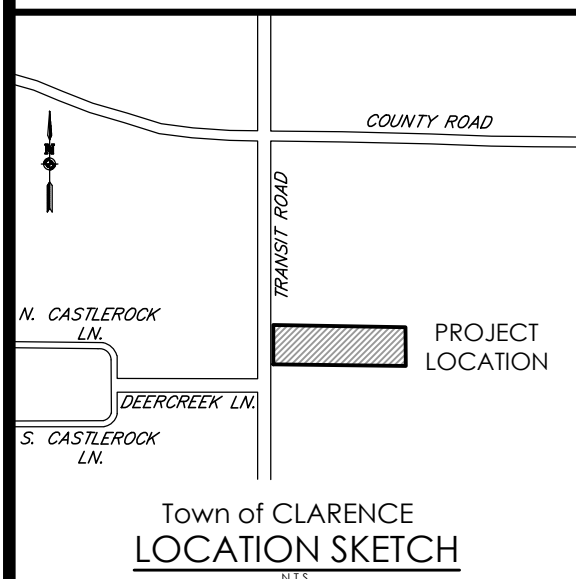
APPROVED SEPTEMBER 18, 2008

/S/ DAVID J. CLEMENTS, P.E.  
DIRECTOR, OFFICE OF TRAFFIC SAFETY AND MOBILITY

ISSUED UNDER EB 08-036

619-50

EFFECTIVE DATE: 01/07/10



Town of CLARENCE

LOCATION SKETCH

Client:

**DANIELE FAMILY COMPANIES**

2851 MONROE AVENUE

ROCHESTER, NY 14618

**PASSERO ASSOCIATES**

242 West Main Street Suite 100  
Rochester, New York 14614

(585) 325-1000  
Fax: (585) 325-1691

Principal-in-Charge: Jess Sudol, PE  
Project Manager: David Cox, PE  
Designed by: James Ritzenhaller



Revisions			
No.	Date	By	Description
1	11/01/21	ABG	PER ECWA COMMENT
2	01/24/22	ABG	PER TOWN COMMENT
3	04/14/22	ABG	PER NYSDOT COMMENTS
4	10/31/22	ABG	FULL ACCESS DRIVEWAY PER NYSDOT
5	12/05/22	ABG	PER TOWN ENGINEER COMMENTS

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#### DETAILS

#### RWD CLARENCE

Town/City: CLARENCE

County: ERIE State: NEW YORK

Project No:

**20213204.0001**

Drawing No. C 207 Sheet No. 15

Scale: N.T.S.

Date: SEPTEMBER 2021

NOT FOR CONSTRUCTION





NTS

NOTE:  
INSPECTION PORTS MAY BE CONNECTED THROUGH ANY CHAMBER CORRUGATION VALLEY

NOT FOR CONSTRUCTION



MC-3500 TECHNICAL SPECIFICATION

NTS

VALLEY STIFFENING RIB

CREST

WEB

LOWER JOINT CORRUGATION

FOOT

UPPER JOINT CORRUGATION

BUILD ROW IN THIS DIRECTION

45.0" (1143 mm)

77.0" (1956 mm)

45.0" (1143 mm)

75.0" (1905 mm)

22.2" (564 mm) INSTALLED

25.7" (653 mm)

**NOMINAL CHAMBER SPECIFICATIONS**

SIZE (W X H X INSTALLED LENGTH)

CHAMBER STORAGE

MINIMUM INSTALLED STORAGE\* WEIGHT

77.0" X 45.0" X 86.0" (3.11 m<sup>3</sup>)

109.9 CUBIC FEET (4.96 m<sup>3</sup>)

175.0 CUBIC FEET (60.8 kg)

**NOMINAL END CAP SPECIFICATIONS**

SIZE (W X H X INSTALLED LENGTH)

END CAP STORAGE

MINIMUM INSTALLED STORAGE\* WEIGHT

75.0" X 45.0" X 22.2" (0.42 m<sup>3</sup>)

14.9 CUBIC FEET (1.28 m<sup>3</sup>)

49 lbs. (22.2 kg)

1956 mm X 1143 mm X 564 mm (3.11 m<sup>3</sup>)

1905 mm X 1143 mm X 564 mm (0.42 m<sup>3</sup>)

1905 mm X 1143 mm X 564 mm (1.28 m<sup>3</sup>)

**\*ASSUMES 12" (305 mm) STONE ABOVE, 6" (229 mm) STONE FOUNDATION, 6" SPACING BETWEEN CHAMBERS, 6" (152 mm) STONE PERIMETER IN FRONT OF END CAPS AND 40% STONE POROSITY**

STUBS AT BOTTOM OF END CAP FOR PART NUMBERS ENDING WITH "B"

STUBS AT TOP OF END CAP FOR PART NUMBERS ENDING WITH "T"

END CAPS WITH A WELDED CROWN PLATE END WITH "C"

END CAPS WITH A PREFABRICATED WELDED STUB END WITH "W"

PART #	STUB	B	C
MC3500IEPP06T	6" (150 mm)	33.21" (844 mm)	---
MC3500IEPP06B	---	31.16" (791 mm)	0.66" (17 mm)
MC3500IEPP08T	8" (200 mm)	---	0.61" (21 mm)
MC3500IEPP08B	---	29.04" (738 mm)	---
MC3500IEPP10T	10" (250 mm)	---	0.59" (24 mm)
MC3500IEPP10B	---	26.36" (670 mm)	---
MC3500IEPP12T	12" (300 mm)	---	1.35" (34 mm)
MC3500IEPP12B	---	23.36" (594 mm)	---
MC3500IEPP15T	15" (375 mm)	---	1.50" (38 mm)
MC3500IEPP15B	---	20.03" (509 mm)	---
MC3500IEPP18T	18" (450 mm)	---	1.77" (45 mm)
MC3500IEPP18B	---	14.46" (368 mm)	---
MC3500IEPP24T	24" (600 mm)	---	2.06" (52 mm)
MC3500IEPP24B	---	---	2.75" (70 mm)
MC3500IEPP24BW	---	---	---
MC3500IEPP30B	30" (750 mm)	---	---

CUSTOM PRECORED INVERTS ARE AVAILABLE UPON REQUEST.

INVENTORED MANIFOLDS INCLUDE 12-24" (300-600 mm) SIZE ON SIZE AND 15-48" (375-1200 mm) ECCENTRIC MANIFOLDS. CUSTOM INVERT LOCATIONS ON THE MC-3500 END CAP CUT IN THE FIELD ARE NOT RECOMMENDED FOR PIPE SIZES GREATER THAN 10" (250 mm). THE INVERT LOCATION IN COLUMN "B" ARE THE HIGHEST POSSIBLE FOR THE PIPE SIZE.

MC-SERIES END CAP INSERTION DETAIL

NTS

MANIFOLD STUB

MANIFOLD HEADER

STORMTECH END CAP

12" (300 mm) MIN SEPARATION

12" (300 mm) MIN INSERTION

12" (300 mm) MIN SEPARATION

12" (300 mm) MIN INSERTION

NOTE: MANIFOLD STUB MUST BE LAID HORIZONTAL FOR A PROPER FIT IN END CAP OPENING.

ROYAL CAR WASH

CLARENCE, NY, USA

DATE: \_\_\_\_\_

DRAWN: IK

CHECKED: N/A

PROJECT # \_\_\_\_\_

DATE: \_\_\_\_\_

DRWN: CHK

DESCRIPTION: \_\_\_\_\_

4840 TRULAMAN BLVD  
HILLIARD, OH 43026  
1-800-735-7473

ADS

StormTech®  
Chamber System

888-862-2894 | WWW.STORMTECH.COM

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SHEET  
5 OF 6

NYLOPLAST DRAIN BASIN

NTS

INTEGRATED DUCTILE IRON FRAME & GRATES/SOLID TO MATCH BASIN O.D.

12" (610 mm) MIN (FOR AASHTO H-20)

18" (457 mm) MIN WIDTH

AASHTO H-20 CONCRETE SLAB 8" (203 mm) MIN THICKNESS

TRAFFIC LOADS, CONCRETE DIMENSIONS ARE FOR GUIDELINE PURPOSES ONLY. ACTUAL CONCRETE SLAB MUST BE DESIGNED GIVING CONSIDERATION FOR LOCAL SOIL CONDITIONS, TRAFFIC LOADING & OTHER APPLICABLE DESIGN FACTORS

ADAPTER ANGLES VARIABLE 0° - 360° ACCORDING TO PLANS

VARIABLE SUMP DEPTH ACCORDING TO PLANS

8" (152 mm) MIN ON 8-24" (200-600 mm), 10" (254 mm) MIN ON 30" (750 mm)]

4" (102 mm) MIN ON 8-24" (200-600 mm) 6" (152 mm) MIN ON 30" (750 mm)

BACKFILL MATERIAL BELOW AND TO SIDES OF STRUCTURE SHALL BE ASTM D2321 CLASS 1 OR 1 CRUSHED STONE OR GRAVEL AND BE PLACED UNIFORMLY IN 12" (305 mm) LIFTS AND COMPACTED TO MIN OF 90%

WATERTIGHT JOINT (CORRUGATED HDPE SHOWN)

VARIOUS TYPES OF INLET AND OUTLET ADAPTERS AVAILABLE: 4-30" (100-750 mm) FOR CORRUGATED HDPE

INVERT ACCORDING TO PLANS/TAKE OFF

INSERTA TEE DETAIL

NTS

CONVEYANCE PIPE MATERIAL MAY VARY (PVC, HDPE, ETC.)

INSERTA TEE CONNECTION

DO NOT INSTALL INSERTA-TEE AT CHAMBER JOINTS

INSERTA TEE TO BE INSTALLED, CENTERED OVER CORRUGATION

PLACE ADSPLUS WOVEN GEOTEXTILE (CENTERED ON INSERTA-TEE INLET), OVER BEDDING STONE FOR SCOUR PROTECTION AT SIDE INLET CONNECTIONS. GEOTEXTILE MUST EXTEND 6" (150 mm) PAST CHAMBER FOOT

SECTION A-A

SIDE VIEW

CHAMBER	MAX DIAMETER OF INSERTA TEE	HEIGHT FROM BASE OF CHAMBER (H)
SC-310	6" (150 mm)	4" (100 mm)
SC-740	10" (250 mm)	4" (100 mm)
DC-780	10" (250 mm)	4" (100 mm)
MC-3500	12" (300 mm)	6" (150 mm)
MC-4500	12" (300 mm)	6" (150 mm)
MC-7200	12" (300 mm)	6" (150 mm)

INSERTA TEE FITTINGS AVAILABLE FOR SDR 26, SDR 35, SCH 40 IPS GASKETED & SOLVENT WELD, N-12, HP STORM, C-900 OR DUCTILE IRON

**NOTES:**

- PART NUMBERS WILL VARY BASED ON INLET PIPE MATERIALS. CONTACT STORMTECH FOR MORE INFORMATION.
- CONTACT ADS ENGINEERING SERVICES IF INSERTA TEE INLET MUST BE RAISED AS NOT ALL INVERTS ARE POSSIBLE.

ROYAL CAR WASH

CLARENCE, NY, USA

DATE: \_\_\_\_\_

DRAWN: IK

CHECKED: N/A

PROJECT # \_\_\_\_\_

DATE: \_\_\_\_\_

DRWN: CHK

DESCRIPTION: \_\_\_\_\_

4840 TRULAMAN BLVD  
HILLIARD, OH 43026  
1-800-735-7473

ADS

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SHEET  
6 OF 6

PA

PASSERO ASSOCIATES

engineering architecture

Client:

DANIELE FAMILY COMPANIES

2851 MONROE AVENUE

ROCHESTER, NY 14618

PROJECT LOCATION

COUNTY ROAD

TRANSIT ROAD

N. CASTLEROCK LN.

S. CASTLEROCK LN.

DEERCREEK LN.

Town of CLARENCE

LOCATION SKETCH

PASSERO ASSOCIATES

242 West Main Street Suite 100  
Rochester, New York 14614

(585) 325-1000  
Fax: (585) 325-1691

Principal-in-Charge: Jess Sudol, PE  
Project Manager: David Cox, PE  
Designed by: James Ritzenhaler

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DETAILS

RWD CLARENCE

Town/City: CLARENCE  
County: ERIE State: NEW YORK

Project No.  
20213204.0001

Drawing No.  
C 209

Sheet No.  
17

Scale:  
N.T.S.

Date  
SEPTEMBER 2021

NOT FOR CONSTRUCTION